

# City of Newton Planning and Development



**77 Hartford Street**

Petition: #111-23

**Special Permit/Site Plan Approval** to allow an additional unit within the existing dwelling, to allow attached single-family dwellings; to reduce the required lot area per unit; to reduce the minimum front setback; to reduce the minimum side setback; to reduce the minimum rear setback; to exceed 36 feet in height; to allow three stories; to waive one parking stall; and to allow a driveway within 10 feet of the side lot line and parking within 20 feet of the side lot line

April 25, 2023

## Zoning Relief

Zoning Relief Required		
<i>Ordinance</i>	<i>Requested Relief</i>	<i>Action Required</i>
§3.4.1	To allow attached single-family dwellings	S.P. per §7.3.3
§3.2.4	To reduce the required lot area per unit	S.P. per §7.3.3
§3.4.2	To reduce minimum front setback	S.P. per §7.3.3
§3.4.2	To reduce minimum side setback	S.P. per §7.3.3
§3.4.2	To reduce minimum rear setback	S.P. per §7.3.3
§3.4.2	To exceed 36 feet in height	S.P. per §7.3.3
§3.2.4	To allow three stories	S.P. per §7.3.3
§5.1.4 §5.1.13	To waive one parking stall	S.P. per §7.3.3
§6.2.3.B.2	To allow a driveway within 10 feet of the side lot line and parking within 20 feet of the side lot line	S.P. per §7.3.3

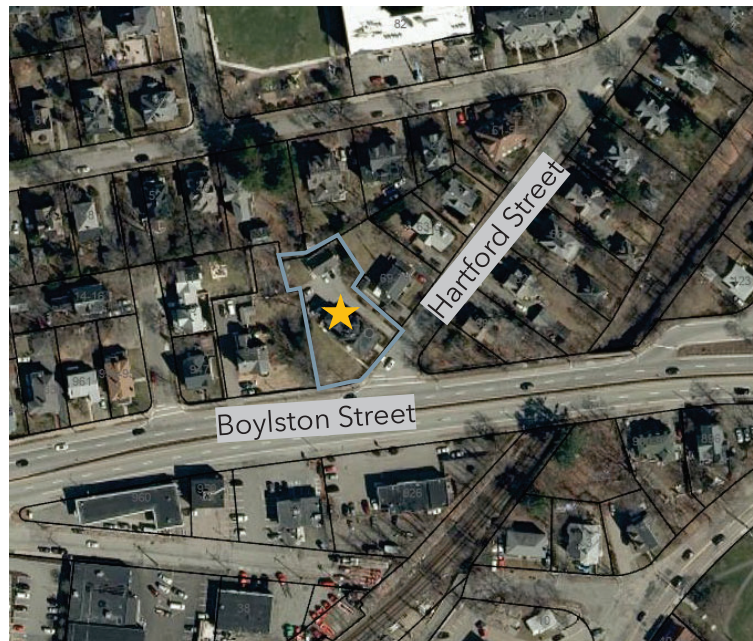
# Criteria to Consider

When reviewing this request, the Council should consider:

- The specific site is an appropriate location for the proposed four attached dwellings in two separate buildings that are 36 feet tall and three stories in height, with reduced lot area per unit, reduced front setback, reduced side setback, reduced rear setback, with one parking stall waived, and with a driveway within 10 feet of the side lot line and parking within 20 feet of the side lot line. (§3.1.3, §3.1.9).
- The proposed project as designed will not adversely affect the neighborhood. (§7.3.3.C.2, §3.1.3, §3.1.9).
- There will not be a nuisance or serious hazard to vehicles or pedestrians (§7.3.3.C.3, §3.1.3, §3.1.9).
- Access to the sites over streets is appropriate for the types and number of vehicles involved (§7.3.3.C.4, §3.1.3, §3.1.9).

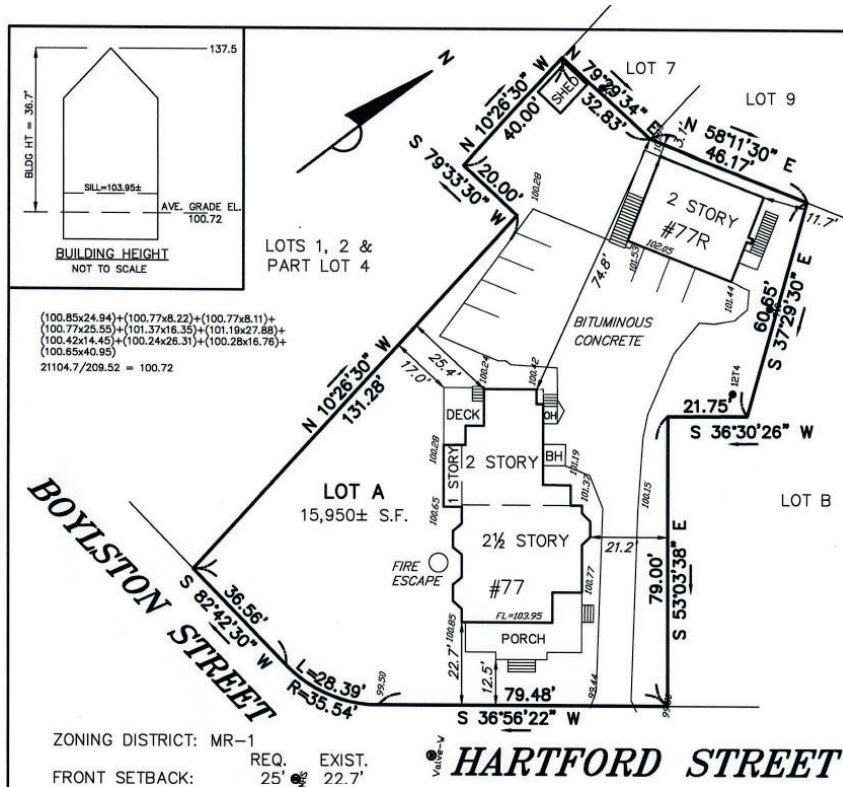
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## Aerial Map



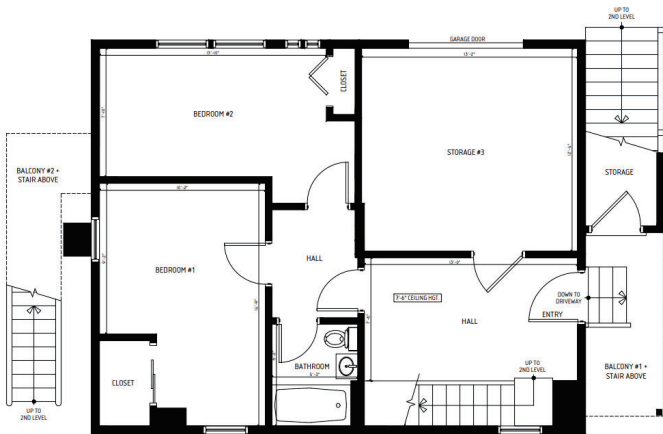
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# Site Plan

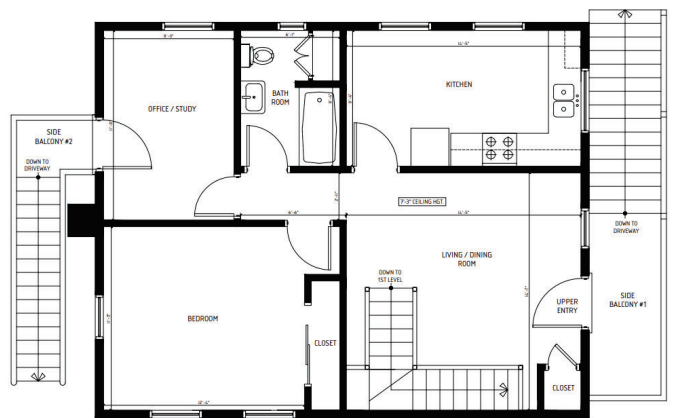


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# Proposed floor plans- Carriagehouse



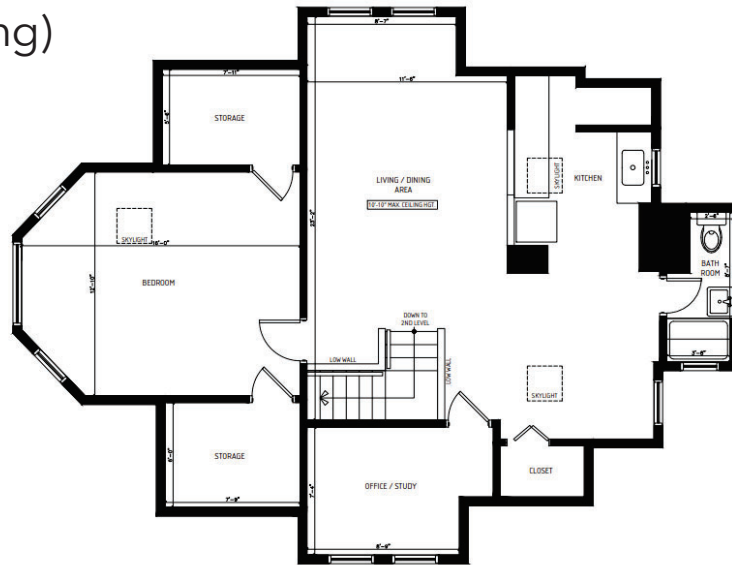
1 PROPOSED ACCESSORY STRUCTURE - 1ST LEVEL FLOOR PLAN  
77 HARTFORD STREET - NEWTON, MA SCALE: 1/4" = 1'-0"



2 PROPOSED ACCESSORY STRUCTURE - 2ND LEVEL FLOOR PLAN  
77 HARTFORD STREET - NEWTON, MA SCALE: 1/4" = 1'-0"

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## Proposed floor plan- new unit (third floor of main building)



3 PROPOSED 3RD LEVEL FLOOR PLAN  
77 HARTFORD STREET - NEWTON, MA SCALE: 1/4" = 1'-0"

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## Proposed findings

1. The specific site in the Multi Residence 1 (MR1) district is an appropriate location for four attached dwellings in two separate buildings that are 36 feet tall and three stories in height, with reduced lot area per unit, reduced front setback, reduced side setback, reduced rear setback, with one parking stall waived, and with a driveway within 10 feet of the side lot line and parking within 20 feet of the side lot line because there is a diverse mix of residential building sizes and configurations in the area. (§7.3.3.C.1)
2. The project as proposed will not adversely affect the neighborhood because the proposed changes are limited to the existing building envelopes. (§7.3.3.C.2)
3. The proposed project will not create a nuisance or serious hazard to vehicles or pedestrians because all parking will be contained on the site and the curb cut will not be altered. (§7.3.3.C.3)
4. Access to the site over streets is appropriate for the types and numbers of vehicles involved. (§7.3.3.C.4)

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## Proposed conditions

1. Plan Referencing & standard conditions
2. Must comply with the City's Short-Term Rental Ordinances