

CITY OF NEWTON DEPARTMENT OF PUBLIC WORKS

PLAN OF THE
RECONSTRUCTION OF THE PEARL STREET PARKING LOT
IN THE CITY OF
NEWTON, MASSACHUSETTS
RUTHANNE FULLER, MAYOR

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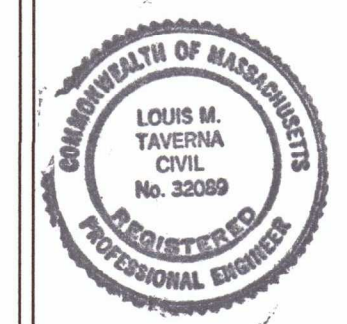
PLAN VIEW
SCALE: 1"=100'

THE CITY OF NEWTON GENERAL CONSTRUCTION DETAILS (CONFORMING TO THE CITY OF NEWTON STANDARD SPECIFICATIONS) REVISED THROUGH DECEMBER 2021, THE COMMONWEALTH OF MASSACHUSETTS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAYS AND BRIDGES DATED 2021 AND AMENDMENTS, THE 2017 CONSTRUCTION STANDARD DETAILS, THE 1996 CONSTRUCTION AND TRAFFIC STANDARD DETAILS (AS RELATES TO TRAFFIC STANDARD DETAILS ONLY), THE 2009 MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS WITH MASSACHUSETTS AMENDMENTS, THE 1968 STANDARD DRAWINGS FOR TRAFFIC SIGNALS AND HIGHWAY LIGHTING AND THE LATEST EDITION OF THE AMERICAN STANDARD FOR NURSERY STOCK SHALL GOVERN.

CITY OF NEWTON
MASSACHUSETTS

DESIGNED BY: A. ZAVARZAS
SURVEYED BY: P. HIGGINS
BASE MAP DRAFTED BY: P. HIGGINS
DESIGN DRAFTED BY: M. PANZA
CHECKED BY: J. D'AGHILIA
APPROVED BY: L. TAVERNA

CITY OF NEWTON
MASSACHUSETTS



LOUIS M. TAVERNA
CIVIL
No. 32099
PROFESSIONAL ENGINEER

TITLE SHEET
FOR THE
RECONSTRUCTION
OF THE
PEARL STREET PARKING LOT

Louis M. Taverna
Louis M. Taverna, P.E.

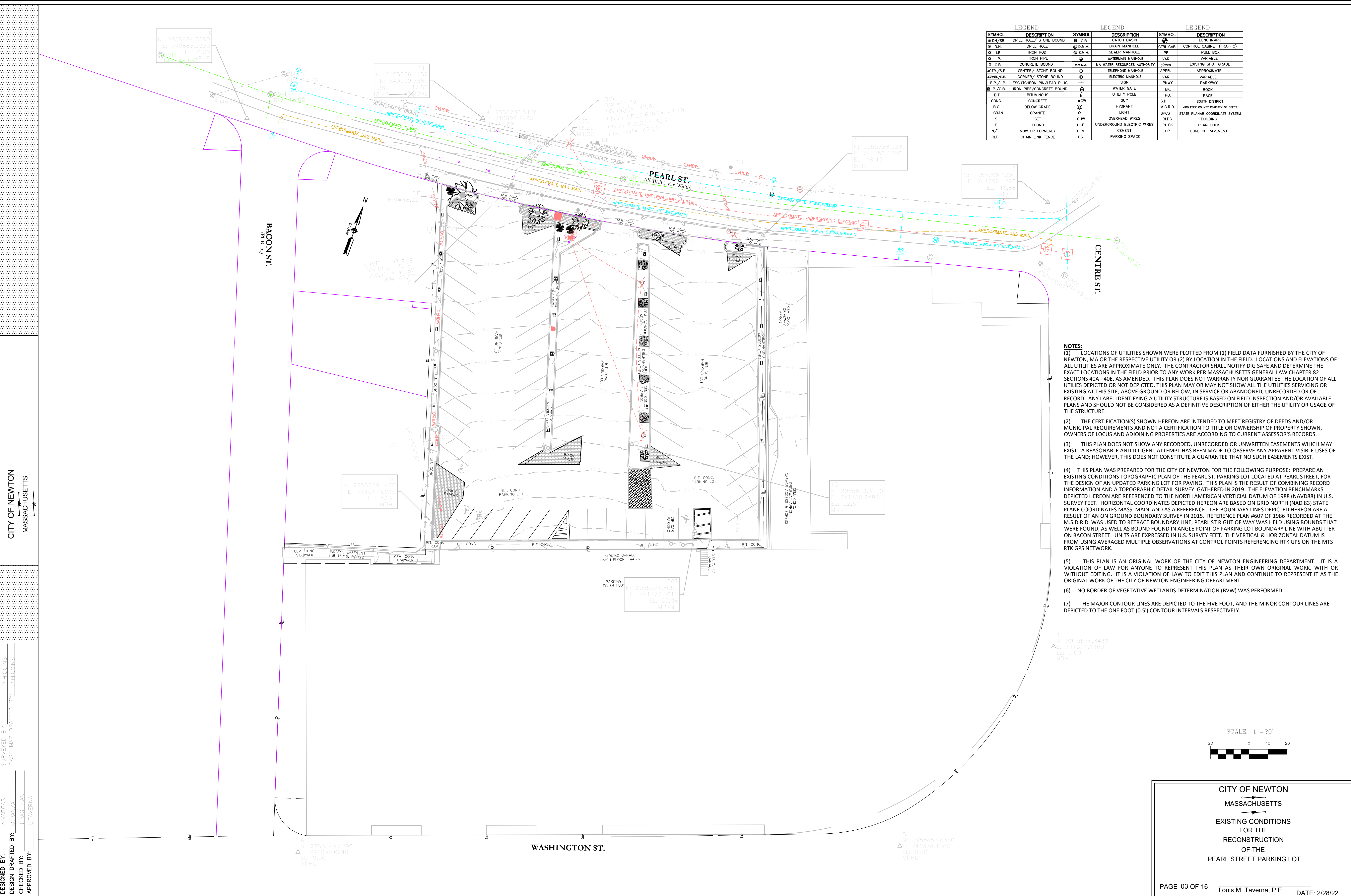
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- 1) THE CONTRACTOR SHALL CALL DIGSAFE AT 1-888-344-7233 AT LEAST 72 HOURS PRIOR TO EXCAVATING AT ANY LOCATION WITHIN THE PROJECT LIMITS, SATURDAYS, SUNDAYS, AND HOLIDAYS EXCLUDED. A COPY OF THE DIGSAFE REFERENCE NUMBER(S) SHALL BE GIVEN TO THE OWNER PRIOR TO EXCAVATION.
- 2) THE LOCATIONS OF EXISTING UNDERGROUND UTILITIES AND OTHER UNDERGROUND OBJECTS ARE SHOWN IN AT APPROXIMATE WAY ONLY AND HAVE NOT BEEN INDEPENDENTLY VERIFIED BY THE OWNER. THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATION OF ALL EXISTING UTILITIES BEFORE COMMENCING ANY WORK AND SHALL BE FULLY RESPONSIBLE FOR ANY AND ALL DAMAGES WHICH MIGHT BE OCCASIONED BY THE CONTRACTORS FAILURE TO EXACTLY LOCATE AND PRESERVE ANY AND ALL UNDERGROUND UTILITIES.
- 3) STONE WALLS, FENCES, MAIL BOXES, SIGNS, CURBS, LIGHT POLES, ETC., SHALL BE REMOVED AND RESET AS NECESSARY TO PERFORM THE WORK.
- 4) ALL PAVEMENT DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPLACED IN ACCORDANCE WITH THE SPECIFICATIONS AND AS SHOWN ON THE DRAWINGS. ALL AREAS DISTURBED BY THE CONTRACTOR BEYOND LIMITS OF PROPOSED WORK SHALL BE RESTORED BY THE CONTRACTOR TO THEIR ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE OWNER.
- 5) THE CONTRACTOR SHALL NOT STORE ANY APPARATUS, MATERIALS, SUPPLIES, AND EQUIPMENT ON DRAINAGE STRUCTURES OR WITHIN 100 FEET OF WETLANDS.
- 6) THE CONTRACTOR SHALL GRADE PROPOSED SLOPES TO MEET EXISTING SLOPES WHERE SHOWN ON PLANS, IN ACCORDANCE WITH THE MINIMUM AND MAXIMUM SLOPES SPECIFIED.
- 7) THE CONTRACTOR SHALL NOTIFY THE CITY OF NEWTON WATER DEPARTMENT AT LEAST 72 HOURS PRIOR TO EXCAVATING NEAR WATERLINES.
- 8) TOPOGRAPHIC INFORMATION FROM THE ACTUAL ON- THE-GROUND INSTRUMENT SURVEY PERFORMED BY THE CITY OF NEWTON ENGINEERING DEPARTMENT IN MAY 2019.
- 9) ALL SAWCUTTING SHALL BE CONSIDERED INCIDENTAL.
- 10) THE CONTRACTOR SHALL ALTER THE MASONRY OF THE TOP SECTION OF ALL EXISTING DRAINAGE STRUCTURES AS NECESSARY FOR CHANGES IN GRADE AND RESET ALL WATER AND DRAINAGE FRAMES, GRATES AND BOXES TO THE PROPOSED FINISHED SURFACE GRADE. REQUIRED NEW MASONRY SHALL BE CLAY BRICK CONFORMING TO M4.05.2.
- 11) THE CONTRACTOR SHALL MAKE ALL ARRANGEMENTS FOR THE ALTERATION AND ADJUSTMENT OF GAS, ELECTRIC, TELEPHONE AND ANY OTHER PRIVATE UTILITIES BY THE UTILITY COMPANIES.
- 12) JOINTS BETWEEN NEW HOT MIX ASPHALT ROADWAY PAVEMENT TOP COURSE AND SAWCUT EXISTING PAVEMENT SHALL BE SEALED WITH HOT Poured RUBBERIZED ASPHALT SEALER.
- 13) THE TERM "PROPOSED" (PROP) MEANS WORK TO BE CONSTRUCTED USING NEW MATERIALS OR, WHERE APPLICABLE, RE-USING EXISTING MATERIALS IN SUITABLE CONDITION IDENTIFIED AS " REMOVE AND RESET (R&R).
- 14) ALL EXISTING GRANITE CURB AND CURB CORNERS REMOVED WITHIN THE LIMITS OF WORK AND IN SUITABLE CONDITION AS DETERMINED BY THE ENGINEER, SHALL BE RE-USED IN THE PROPOSED WORK, EXCEPT CURVED STONES OF DIFFERENT RADIUS THAN PROPOSED CURB. ALL CURB NOT USED SHALL BE REMOVED AND DISCARDED.

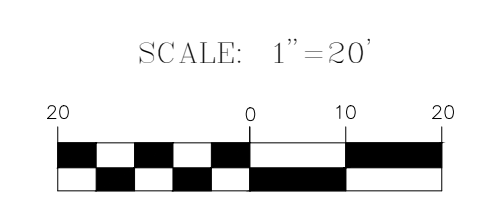
TRAFFIC & PROJECT ZONE INFORMATION

1. ALL CONSTRUCTION SIGNING, TEMPORARY TRAFFIC CONTROL DEVICES AND ROADSIDE ELEMENTS SHALL CONFORM WITH THE 2009 MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AS AMENDED, THE MASSDOT STANDARD DETAILS AND DRAWINGS FOR THE DEVELOPMENT OF TEMPORARY TRAFFIC CONTROL PLANS, THE LATEST REVISIONS OF THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO) ROADSIDE DESIGN GUIDE, AASHTO POLICY ON GEOMETRIC DESIGN OF HIGHWAYS AND STREETS AND NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 OR THE AASHTO MANUAL FOR ASSESSING SAFETY HARDWARE (MASH).
2. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR ESTABLISHING DAILY DETOUR ROUTES TO THE SATISFACTION OF THE ENGINEER AND NEWTON POLICE DEPARTMENT.
3. NO DETOURS WILL BE ALLOWED UNLESS APPROVED AND/OR DIRECTED BY THE ENGINEER AND NEWTON POLICE, BUT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO BOTH ESTABLISH AND MAINTAIN SUCH DETOUR ROUTES.
4. THE ROAD IS NOT TO BE CLOSED TO TRAFFIC AT ANY TIME UNLESS OTHERWISE ORDERED TO DO SO BY THE ENGINEER AND/OR NEWTON POLICE.
5. NON-EMERGENCY DETOURS MUST BE APPROVED BY THE CITY OF NEWTON TRAFFIC ENGINEER AT LEAST 24 HOURS IN ADVANCE OF THE EVENT. TO THAT END THE CONTRACTOR IS TO PROVIDE A HARD-COPY TRAFFIC CONTROL PLAN TO THE TRAFFIC ENGINEER FOR COMMENTARY AND/OR APPROVAL.
6. ALL DETOURS SHALL BE CLEARLY MARKED ALONG THEIR ENTIRE ROUTE, AND ONLY WITH APPROVED DEVICES AND SIGNS, SUCH THAT A STEADY FLOW OF TRAFFIC IS SAFELY MAINTAINED AT ALL TIMES.
7. BUS TRAFFIC SHALL NOT BE DISRUPTED (I.E. SCHOOL BUSES AND PUBLIC TRANSPORTATION SCHEDULES).
8. TRAFFIC DELAYS (IN ANY DIRECTION) ARE NOT TO EXCEED FOUR MINUTES IN DURATION.
9. TEMPORARY PATCH MUST BE INSTALLED AT THE END OF EVERY WORKDAY WHEN TRENCHING HAS BEEN PERFORMED WITHIN THE ROADWAY.
10. ADDITIONAL SIGNS, DRUMS AND OTHER TRAFFIC CONTROL DEVICES, GRADING AND TEMPORARY PAVEMENT FOR PASSAGE OF PEDESTRIAN, VEHICLE AND EMERGENCY TRAFFIC THROUGH THE WORK AREAS, BOTH DURING AND AFTER WORKING HOURS MAY BE REQUIRED TO MAINTAIN ACCESS.
11. CONTRACTOR SHALL SECURE WORK AREAS TO PREVENT UNAUTHORIZED ACCESS AT ALL TIMES.

LEGEND		LEGEND		LEGEND	
SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION	SYMBOL	DESCRIPTION
⊕ D.H./S.B.	DRILL HOLE/STONE BOUND	⊕ C.B.	CATCH BASIN	⊕ BENCHM.	BENCHMARK
⊕ D.H.	DRILL HOLE	⊕ D.M.H.	DRAIN MANHOLE	⊕ CTRL.CAB.	CONTROL CABINET (TRAFFIC)
⊕ I.R.	IRON ROD	⊕ S.M.H.	SEWER MANHOLE	⊕ PB	PULL BOX
⊕ I.P.	IRON PIPE	⊕ W.M.H.	WATERMAIN MANHOLE	⊕ VAR.	VARIABLE
⊕ C.B.	CONCRETE BOUND	⊕ M.W.R.A.	MA WATER RESOURCES AUTHORITY	⊕ EXISTING SPOT GRADE	EXISTING SPOT GRADE
⊕ CTR./S.B.	CENTER/STONE BOUND	⊕ TEL.M.H.	TELEPHONE MANHOLE	⊕ APPR.	APPROXIMATE
⊕ CORN./S.B.	CORNER/STONE BOUND	⊕ E.M.H.	ELECTRIC MANHOLE	⊕ VAR.	VARIABLE
⊕ E.P./A.P.	ESCUTCHEON PIN/LEAD PLUG	⊕ SIGN	SIGN	⊕ PKWY.	PARKWAY
⊕ I.P./C.B.	IRON PIPE/CONCRETE BOUND	⊕ WATER GATE	WATER GATE	⊕ BK.	BOOK
⊕ BIT.	BITUMINOUS	⊕ UTILITY POLE	UTILITY POLE	⊕ PG.	PAGE
⊕ CONC.	CONCRETE	⊕ GUY	GUY	⊕ S.D.	SOUTH DISTRICT
⊕ B.G.	BELOW GRADE	⊕ HYDRANT	HYDRANT	⊕ M.C.R.D.	MIDDLESEX COUNTY REGISTRY OF DEEDS
⊕ GRAN.	GRANITE	⊕ LIGHT	LIGHT	⊕ SPCS	STATE PLANNING COORDINATE SYSTEM
⊕ S.	SET	⊕ OHW	OVERHEAD WIRES	⊕ BLDG.	BUILDING
⊕ F.	FOUND	⊕ UGE	UNDERGROUND ELECTRIC WIRES	⊕ PL.BK.	PLAN BOOK
⊕ N/F	NOW OR FORMERLY	⊕ CEM.	CEMENT	⊕ ECP	EDGE OF PAVEMENT
⊕ CLF	CHAIN LINK FENCE	⊕ PS	PARKING SPACE		



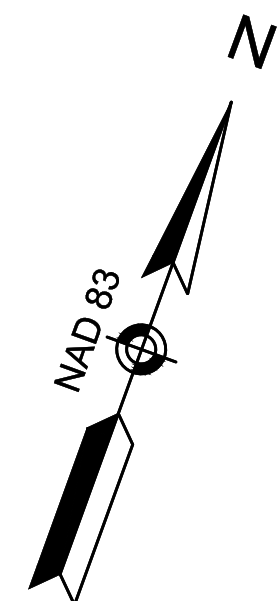
- NOTES:**
- (1) LOCATIONS OF UTILITIES SHOWN ARE PLOTTED FROM (1) FIELD DATA FURNISHED BY THE CITY OF NEWTON, MA OR THE RESPECTIVE UTILITY OR (2) BY LOCATION IN THE FIELD. LOCATIONS AND ELEVATIONS OF ALL UTILITIES ARE APPROXIMATE ONLY. THE CONTRACTOR SHALL NOTIFY DIG SAFE AND DETERMINE THE EXACT LOCATIONS IN THE FIELD PRIOR TO ANY WORK PER MASSACHUSETTS GENERAL LAW CHAPTER 82 SECTIONS 40A-40E, AS AMENDED. THIS PLAN DOES NOT WARRANT NOR GUARANTEE THE LOCATION OF ALL UTILITIES DEPICTED OR NOT DEPICTED. THIS PLAN MAY OR MAY NOT SHOW ALL THE UTILITIES SERVICING OR EXISTING AT THIS SITE; ABOVE GROUND OR BELOW, IN SERVICE OR ABANDONED, UNRECORDED OR OF RECORD. ANY LABEL IDENTIFYING A UTILITY STRUCTURE IS BASED ON FIELD INSPECTION AND/OR AVAILABLE PLANS AND SHOULD NOT BE CONSIDERED AS A DEFINITIVE DESCRIPTION OF EITHER THE UTILITY OR USAGE OF THE STRUCTURE.
 - (2) THE CERTIFICATION(S) SHOWN HEREON ARE INTENDED TO MEET REGISTRY OF DEEDS AND/OR MUNICIPAL REQUIREMENTS AND NOT A CERTIFICATION TO TITLE OR OWNERSHIP OF PROPERTY SHOWN. OWNERS OF LOCUS AND ADJOINING PROPERTIES ARE ACCORDING TO CURRENT ASSESSOR'S RECORDS.
 - (3) THIS PLAN DOES NOT SHOW ANY RECORDED, UNRECORDED OR UNWRITTEN EASEMENTS WHICH MAY EXIST. A REASONABLE AND DILIGENT ATTEMPT HAS BEEN MADE TO OBSERVE ANY APPARENT VISIBLE USES OF THE LAND; HOWEVER, THIS DOES NOT CONSTITUTE A GUARANTEE THAT NO SUCH EASEMENTS EXIST.
 - (4) THIS PLAN WAS PREPARED FOR THE CITY OF NEWTON FOR THE FOLLOWING PURPOSE: PREPARE AN EXISTING CONDITIONS TOPOGRAPHIC PLAN OF THE PEARL ST. PARKING LOT LOCATED AT PEARL STREET, FOR THE DESIGN OF AN UPDATED PARKING LOT FOR PAVING. THIS PLAN IS THE RESULT OF COMBINING RECORD INFORMATION AND A TOPOGRAPHIC DETAIL SURVEY GATHERED IN 2019. THE ELEVATION BENCHMARKS DEPICTED HEREON ARE REFERENCED TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88) IN U.S. SURVEY FEET. HORIZONTAL COORDINATES DEPICTED HEREON ARE BASED ON GRID NORTH (NAD 83) STATE PLANE COORDINATES MASS. MAINLAND AS A REFERENCE. THE BOUNDARY LINES DEPICTED HEREON ARE A RESULT OF AN ON GROUND BOUNDARY SURVEY IN 2015. REFERENCE PLAN #607 OF 1986 RECORDED AT THE M.S.D.R.D. WAS USED TO RETRACE BOUNDARY LINE. PEARL ST RIGHT OF WAY WAS HELD USING BOUNDS THAT WERE FOUND, AS WELL AS BOUND FOUND IN ANGLE POINT OF PARKING LOT BOUNDARY LINE WITH ABUTTER ON BACON STREET. UNITS ARE EXPRESSED IN U.S. SURVEY FEET. THE VERTICAL & HORIZONTAL DATUM IS FROM USING AVERAGED MULTIPLE OBSERVATIONS AT CONTROL POINTS REFERENCING RTK GPS ON THE MTS RTK GPS NETWORK.
 - (5) THIS PLAN IS AN ORIGINAL WORK OF THE CITY OF NEWTON ENGINEERING DEPARTMENT. IT IS A VIOLATION OF LAW FOR ANYONE TO REPRESENT THIS PLAN AS THEIR OWN ORIGINAL WORK, WITH OR WITHOUT EDITING. IT IS A VIOLATION OF LAW TO EDIT THIS PLAN AND CONTINUE TO REPRESENT IT AS THE ORIGINAL WORK OF THE CITY OF NEWTON ENGINEERING DEPARTMENT.
 - (6) NO BORDER OF VEGETATIVE WETLANDS DETERMINATION (BVW) WAS PERFORMED.
 - (7) THE MAJOR CONTOUR LINES ARE DEPICTED TO THE FIVE FOOT, AND THE MINOR CONTOUR LINES ARE DEPICTED TO THE ONE FOOT (0.5') CONTOUR INTERVALS RESPECTIVELY.



CITY OF NEWTON
 MASSACHUSETTS
 EXISTING CONDITIONS
 FOR THE
 RECONSTRUCTION
 OF THE
 PEARL STREET PARKING LOT

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 Louis M. Taverna, P.E.
 DATE: 2/28/22

BACON ST.

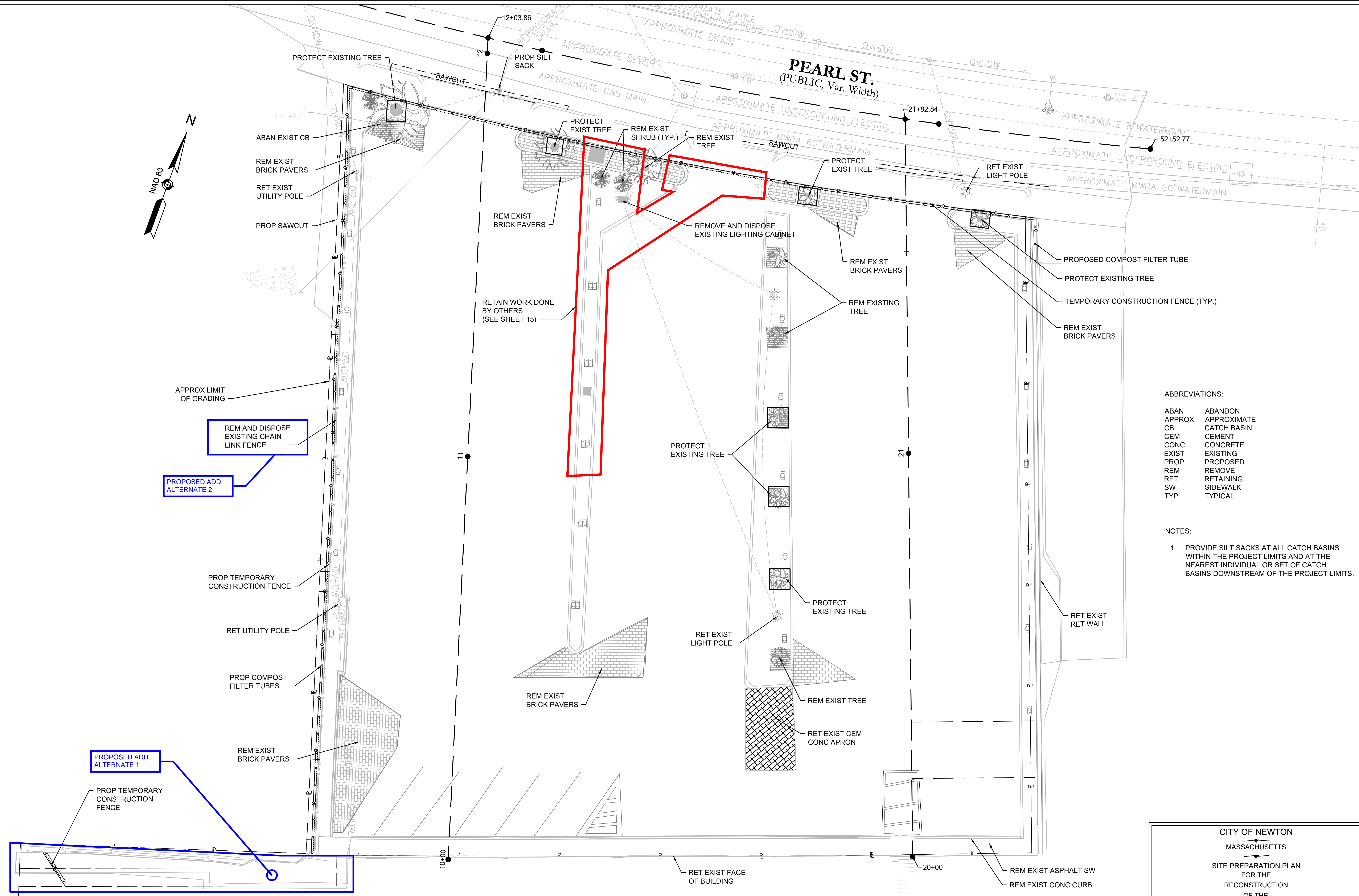


PEARL ST.
(PUBLIC, Var. Width)

CITY OF NEWTON
MASSACHUSETTS

DESIGNED BY: A. VERGAS
CHECKED BY: M. PANZA
APPROVED BY: L. TAVERNA

BASE MAP DRAFTED BY: P. HIGGINS



APPROX LIMIT OF GRADING

REM AND DISPOSE EXISTING CHAIN LINK FENCE

PROPOSED ADD ALTERNATE 2

PROPOSED ADD ALTERNATE 1

ABBREVIATIONS:

ABAN	ABANDON
APPROX	APPROXIMATE
CB	CATCH BASIN
CEM	CEMENT
CONC	CONCRETE
EXIST	EXISTING
PROP	PROPOSED
REM	REMOVE
RET	RETAINING
SW	SIDEWALK
TYP	TYPICAL

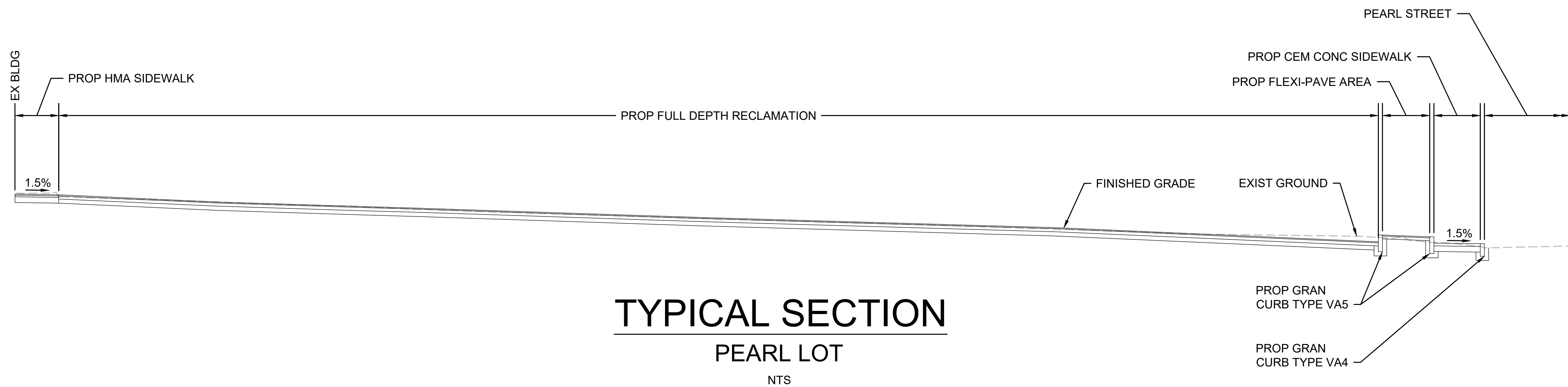
- NOTES:**
1. PROVIDE SILT SACKS AT ALL CATCH BASINS WITHIN THE PROJECT LIMITS AND AT THE NEAREST INDIVIDUAL OR SET OF CATCH BASINS DOWNSTREAM OF THE PROJECT LIMITS.



CITY OF NEWTON
MASSACHUSETTS

SITE PREPARATION PLAN
FOR THE
RECONSTRUCTION
OF THE
PEARL STREET PARKING LOT

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TYPICAL SECTION
PEARL LOT
NTS

PAVEMENT NOTES

PROPOSED FULL DEPTH RECLAMATION

SURFACE: 1.5" SUPERPAVE SURFACE COURSE - 12.5 (SSC-12.5) OVER
 4.5" SUPERPAVE INTERMEDIATE COURSE 19.0 (SIC-19.0)
 PLACED IN TWO 2.25" LAYERS OVER
 SUBBASE: 6" RECLAIMED PAVEMENT

PROPOSED HOT MIX ASPHALT SIDEWALK

SURFACE: 1.5" SUPERPAVE SURFACE COURSE - 12.5 (SSC-12.5) OVER
 2.25" SUPERPAVE INTERMEDIATE COURSE - 19.0 (SIC-19.0) OVER
 ++SUBBASE: 8.25" RECLAIMED PAVEMENT

PROPOSED CEMENT CONCRETE SIDEWALK

SURFACE: 4" CEMENT CONCRETE
 4000 PSI, 3/4", 610 (PER MASSDOT STD SPEC M4.02.00)
 ++SUBBASE: 6" GRAVEL BORROW, TYPE B
 2" DENSE GRADED CRUSHED STONE TO LEVEL

PROPOSED CEMENT CONCRETE DRIVEWAY APRON

SURFACE: 6" CEMENT CONCRETE
 4000 PSI, 3/4", 610 (PER MASSDOT STD SPEC M4.02.00)
 ++SUBBASE: 8" GRAVEL BORROW, TYPE B
 2" DENSE GRADED CRUSHED STONE TO LEVEL

PROPOSED POROUS PAVEMENT

SURFACE: 1.5" POROUS PAVEMENT
 BASE: 4" CRUSHED STONE BASE
 ++SUBBASE: 6" BANK GRAVEL, TYPE A

PROPOSED GRASS STRIP / LOAM AND SEED

SURFACE: 6" LOAM AND SEED
 ++SUBBASE: 6" BANK GRAVEL, TYPE A

PROPOSED GRAVEL STRIP

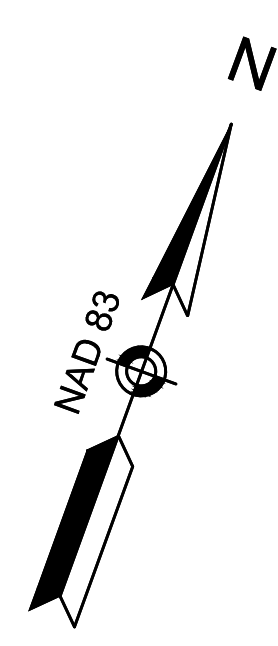
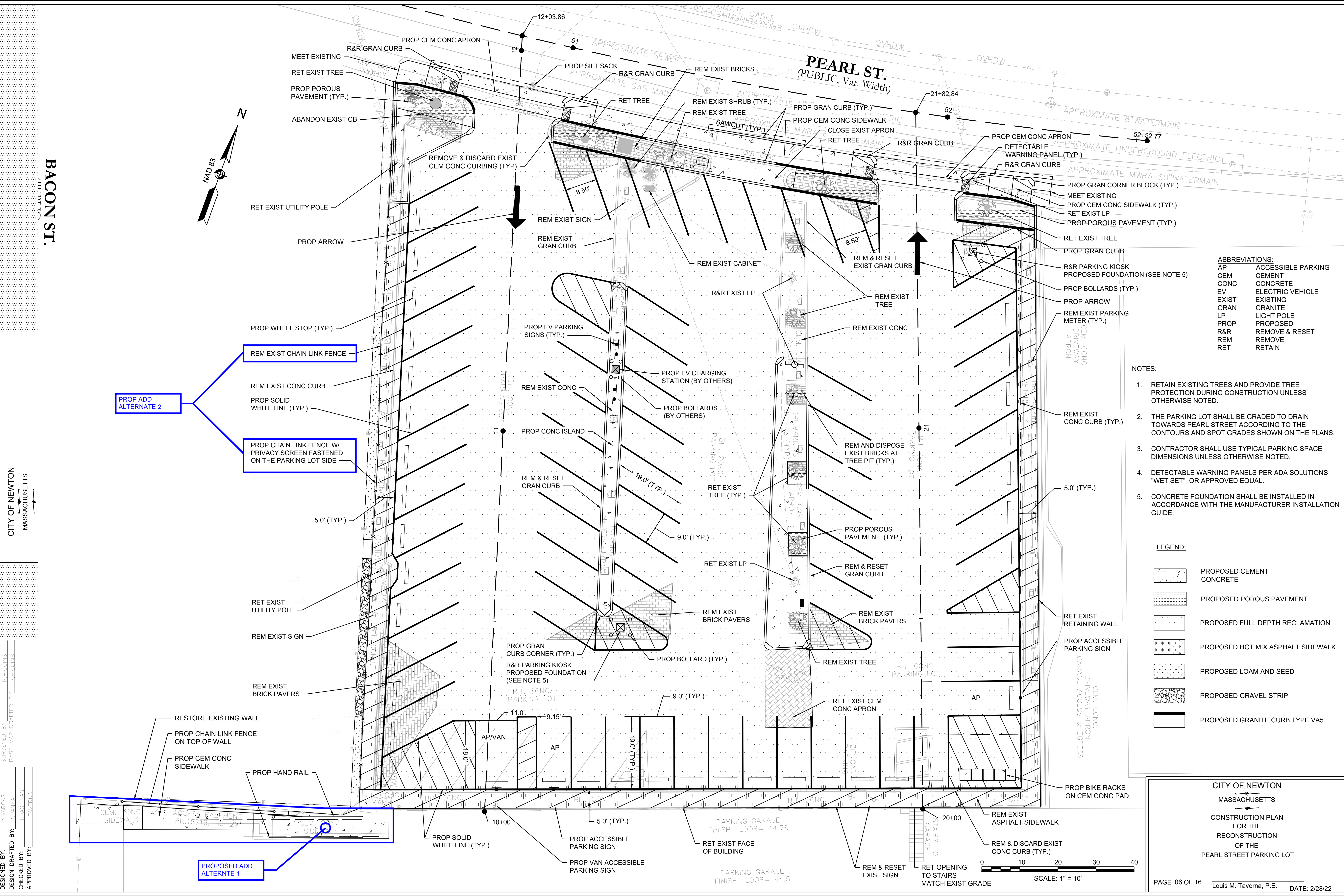
SURFACE: 6" GRAVEL BORROW, TYPE B

++ WHERE EXISTING GRAVEL IS FOUND TO BE SUITABLE, THE EXISTING GRAVEL MAY BE USED IN PROPOSED SUBBASE, AFTER APPROVAL BY THE ENGINEER.

- NOTES:
- ALL HOT MIX ASPHALT PAVEMENTS SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 450 HOT MIX ASPHALT PAVEMENT SPECIFICATIONS.
 - ALL MILLED SURFACES SHALL RECEIVE A TACK COAT APPLIED AT 0.07 to 0.09 GALLONS PER SQUARE YARD AND ALL UNMILLED SURFACES SHALL RECEIVE A TACK COAT APPLIED AT 0.06 to 0.08 GALLONS PER SQUARE YARD PRIOR TO PAVING.
 - HOT MIX ASPHALT SIDEWALK, DRIVEWAY AND TEMPORARY CONSTRUCTION SHALL USE SUPERPAVE HOT MIX ASPHALT MIXTURES AND MAY NOT BE SUBJECT TO THE SAME SAMPLING AND TESTING REQUIRED FOR PERMANENT CONSTRUCTION.

CITY OF NEWTON
 MASSACHUSETTS
 TYPICAL SECTION
 FOR THE
 RECONSTRUCTION
 OF THE
 PEARL STREET PARKING LOT

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BACON ST.

PEARL ST.
(PUBLIC, Var. Width)

ABBREVIATIONS:

AP	ACCESSIBLE PARKING
CEM	CEMENT
CONC	CONCRETE
EV	ELECTRIC VEHICLE
EXIST	EXISTING
GRAN	GRANITE
LP	LIGHT POLE
PROP	PROPOSED
R&R	REMOVE & RESET
REM	REMOVE
RET	RETAIN

- NOTES:
1. RETAIN EXISTING TREES AND PROVIDE TREE PROTECTION DURING CONSTRUCTION UNLESS OTHERWISE NOTED.
 2. THE PARKING LOT SHALL BE GRADED TO DRAIN TOWARDS PEARL STREET ACCORDING TO THE CONTOURS AND SPOT GRADES SHOWN ON THE PLANS.
 3. CONTRACTOR SHALL USE TYPICAL PARKING SPACE DIMENSIONS UNLESS OTHERWISE NOTED.
 4. DETECTABLE WARNING PANELS PER ADA SOLUTIONS "WET SET" OR APPROVED EQUAL.
 5. CONCRETE FOUNDATION SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER INSTALLATION GUIDE.

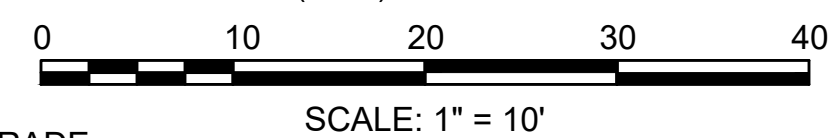
LEGEND:

[Pattern]	PROPOSED CEMENT CONCRETE
[Pattern]	PROPOSED POROUS PAVEMENT
[Pattern]	PROPOSED FULL DEPTH RECLAMATION
[Pattern]	PROPOSED HOT MIX ASPHALT SIDEWALK
[Pattern]	PROPOSED LOAM AND SEED
[Pattern]	PROPOSED GRAVEL STRIP
[Pattern]	PROPOSED GRANITE CURB TYPE VA5

CITY OF NEWTON
MASSACHUSETTS

DESIGNED BY: A. VERGARA
 DESIGN DRAFTED BY: M. PANZA
 CHECKED BY: M. DACHIAN
 APPROVED BY: L. LAVERNA

CITY OF NEWTON
MASSACHUSETTS
CONSTRUCTION PLAN
FOR THE
RECONSTRUCTION
OF THE
PEARL STREET PARKING LOT



PROPOSED ADD
ALTERTE 1

PROPOSED ADD
ALTERNATE 2

PROPOSED CHAIN LINK FENCE W/
PRIVACY SCREEN FASTENED
ON THE PARKING LOT SIDE

REMOVE EXIST CHAIN LINK FENCE

REMOVE EXIST CONC CURB

PROPOSED SOLID WHITE LINE (TYP.)

5.0' (TYP.)

RET EXIST UTILITY POLE

REMOVE EXIST SIGN

REMOVE EXIST BRICK PAVERS

RESTORE EXISTING WALL

PROPOSED CHAIN LINK FENCE ON TOP OF WALL

PROPOSED CEM CONC SIDEWALK

PROPOSED HAND RAIL

PROPOSED SOLID WHITE LINE (TYP.)

PROPOSED VAN ACCESSIBLE PARKING SIGN

PROPOSED EV PARKING SIGNS (TYP.)

REMOVE EXIST CONC

PROPOSED CONC ISLAND

REMOVE & RESET GRAN CURB

19.0' (TYP.)

9.0' (TYP.)

REMOVE EXIST LP

REMOVE EXIST BRICK PAVERS

PROPOSED GRAN CURB CORNER (TYP.)

R&R PARKING KIOSK PROPOSED FOUNDATION (SEE NOTE 5)

BIT. CONC. PARKING LOT

11.0'

9.15'

AP/VAN

AP

19.0' (TYP.)

5.0' (TYP.)

PROPOSED EV CHARGING STATION (BY OTHERS)

PROPOSED BOLLARDS (BY OTHERS)

REMOVE & DISPOSE EXIST BRICKS AT TREE PIT (TYP.)

REMOVE EXIST CONC

REMOVE EXIST TREE

PROPOSED POROUS PAVEMENT (TYP.)

REMOVE & RESET GRAN CURB

REMOVE EXIST BRICK PAVERS

REMOVE EXIST TREE

REMOVE EXIST LP

REMOVE EXIST CEM CONC APRON

9.0' (TYP.)

RET EXIST FACE OF BUILDING

PARKING GARAGE FINISH FLOOR= 44.76

RET EXIST FACE OF BUILDING

PARKING GARAGE FINISH FLOOR= 44.5

REMOVE & RESET EXIST SIGN

REMOVE EXIST CONC

REMOVE EXIST TREE

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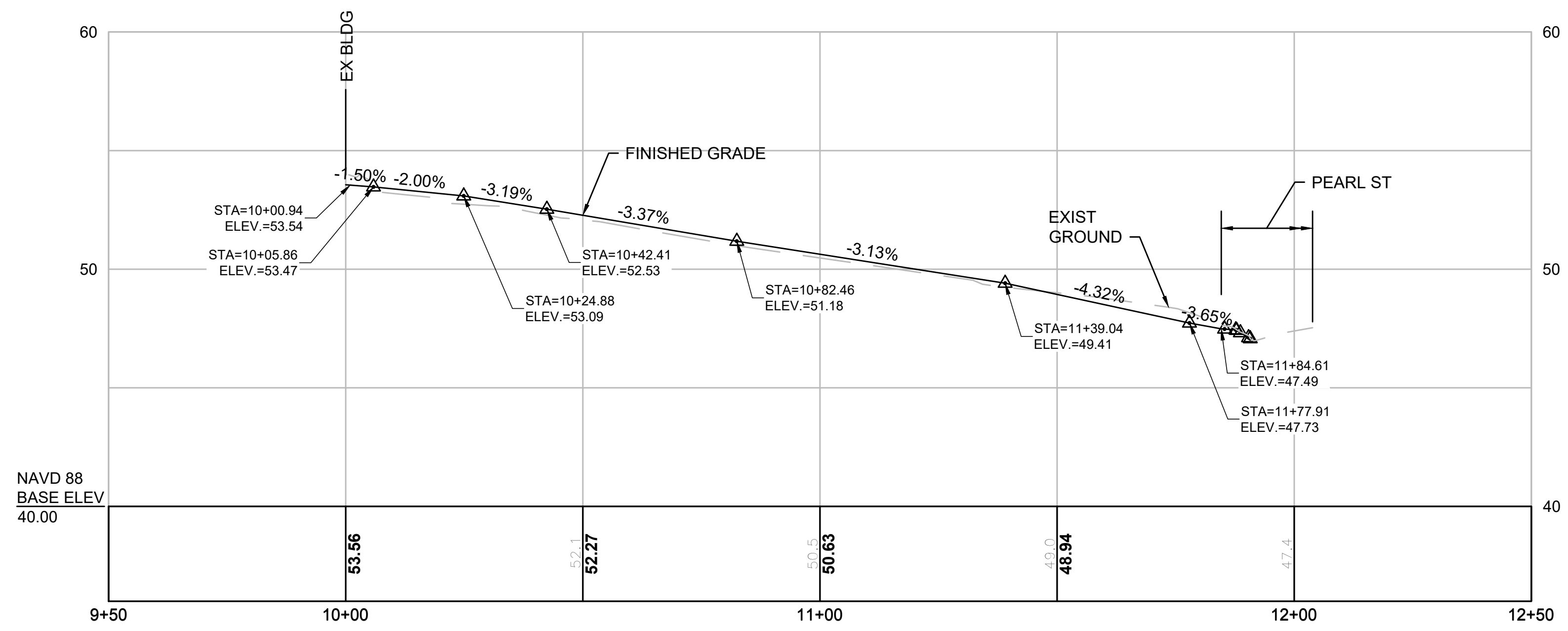
REMOVE EXIST CONC

REMOVE EXIST CONC

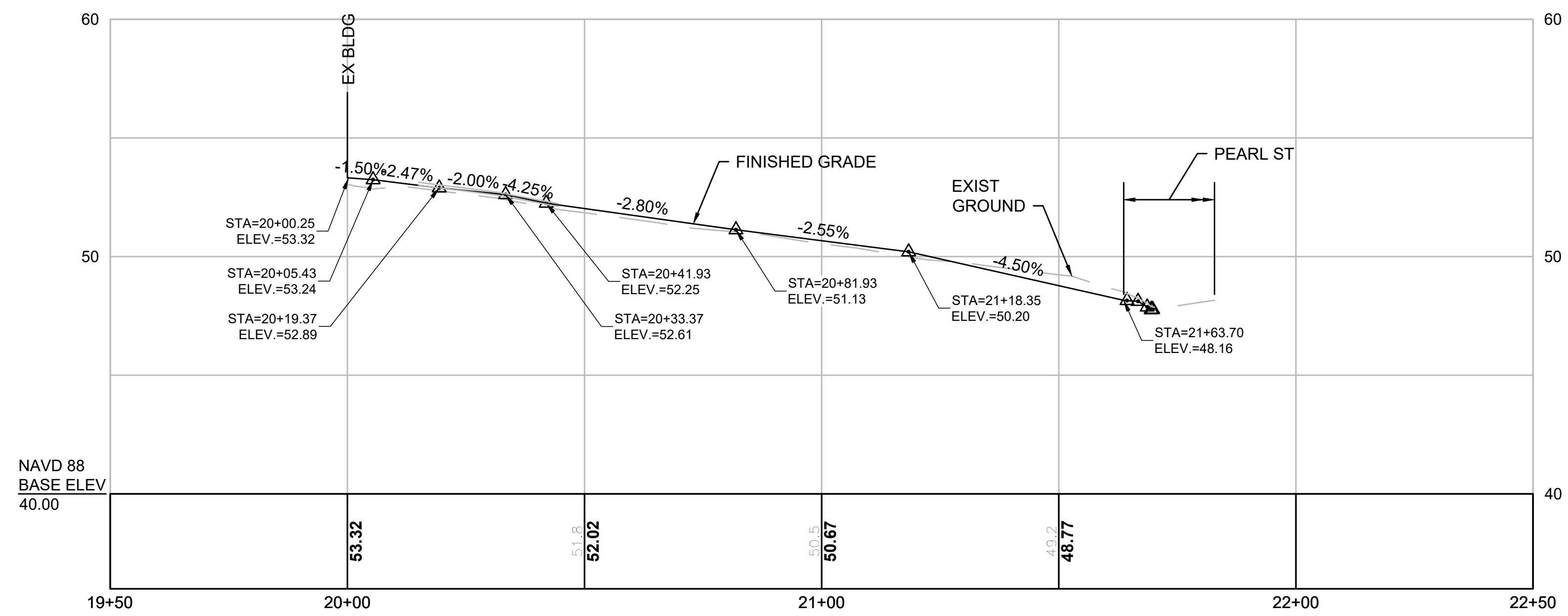
REMOVE EXIST CONC

REMOVE EXIST CONC

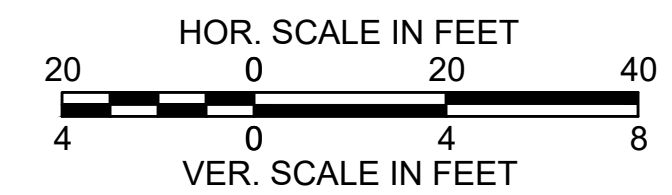
REMOVE EXIST CONC



ALIGN - PEARL LOT LEFT ISLE

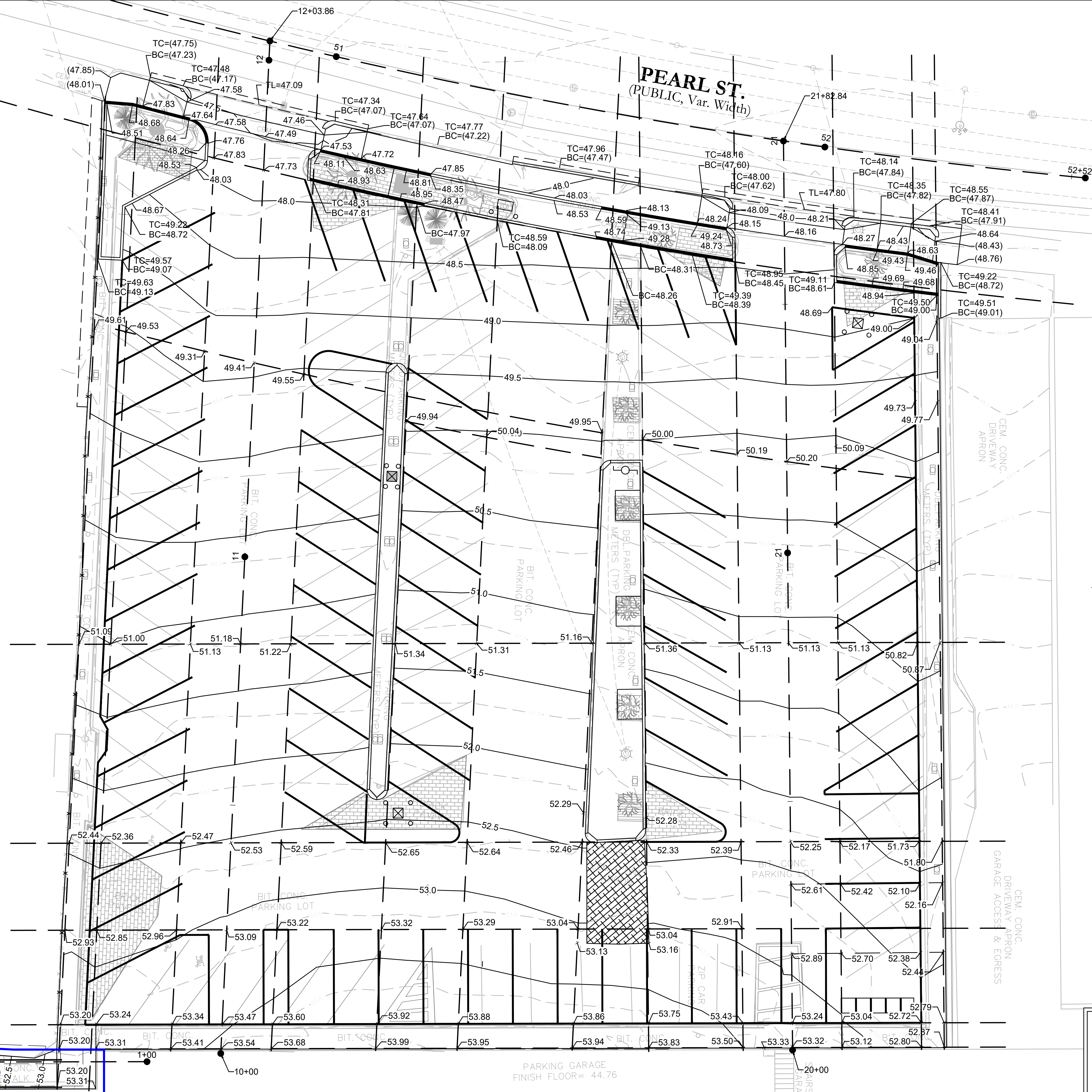
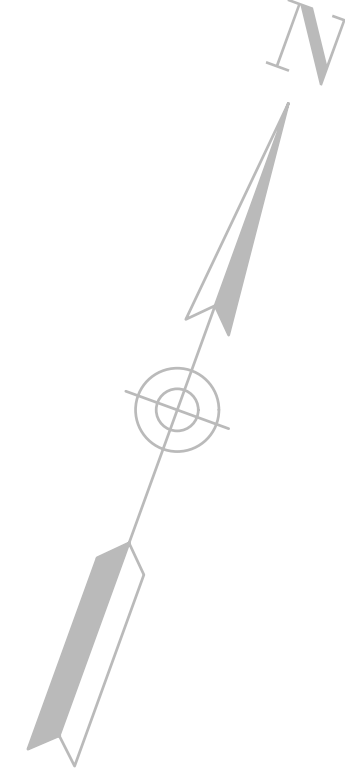


ALIGN - PEARL LOT RIGHT ISLE



BACON ST.

PEARL ST.
(PUBLIC, Var. Width)



ABBREVIATIONS:

- BL BOTTOM OF LIP
- BC BOTTOM OF CURB
- GB GRADE BREAK
- HP HIGH POINT
- LP LOW POINT
- TB TOP OF BERM
- TC TOP OF CURB
- TL TOP OF LIP
- TYP TYPICAL

LEGEND:

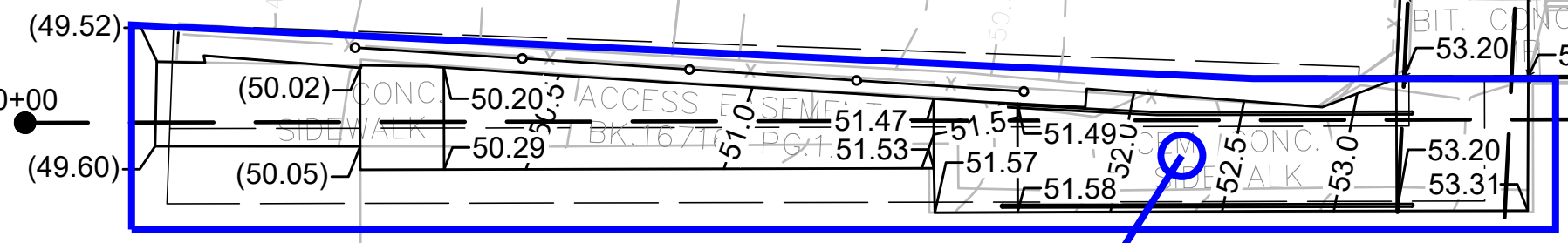
- XXX.XX PROPOSED SPOT ELEVATION
- (XXX.XX) EXISTING SPOT ELEVATION
- PROP GRADE BREAK LINE

NOTES:

1. SEE PROFILES FOR ADDITIONAL GRADING INFORMATION.

CITY OF NEWTON
MASSACHUSETTS

DESIGNED BY: A. VERGAS
 DESIGN DRAFTED BY: M. PANZA
 CHECKED BY: M. DACHIAN
 APPROVED BY: L. LAVERNA



PROPOSED ADD
ALTERNATE 1



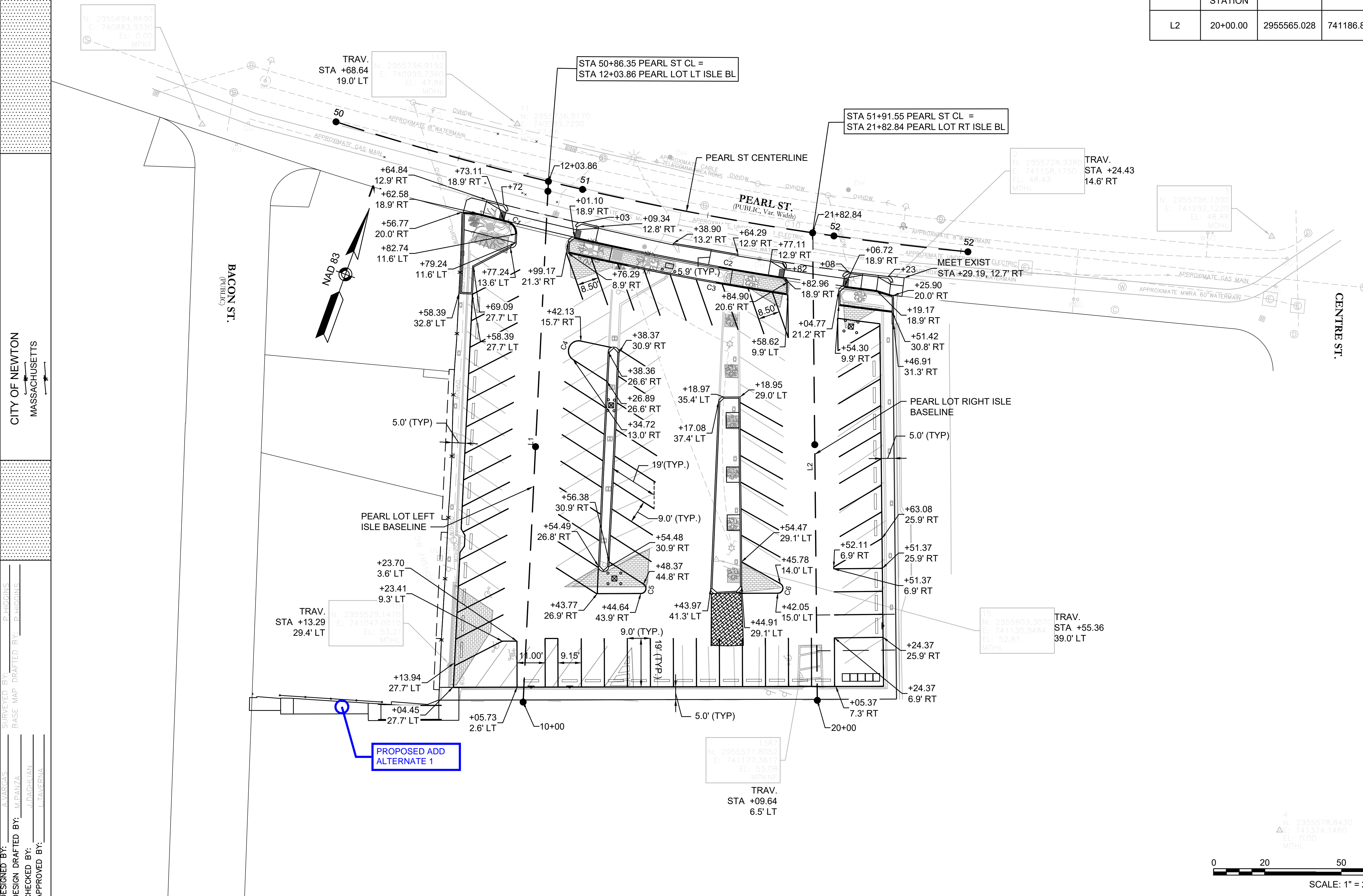
CITY OF NEWTON
MASSACHUSETTS
GRADING PLAN
FOR THE
RECONSTRUCTION
OF THE
PEARL STREET PARKING LOT

ALIGN - PEARL ST CL CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
C10	50+00.00	2955713.589	740932.818	R= 1180.00' Δ= 12°16'24" L=252.77' T=126.87'		52+52.77	2955750.003	741182.458

ALIGN - PEARL LOT LEFT ISLE CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L1	10+00.00	2955525.187	741078.938		N17°08'37"W 203.86'	12+03.86	2955719.993	741018.846

ALIGN - PEARL LOT RIGHT ISLE CONSTRUCTION BASELINE DATA								
NUMBER	STARTING STATION	NORTHING	EASTING	CURVE DATA	LINE DATA	ENDING STATION	NORTHING	EASTING
L2	20+00.00	2955565.028	741186.862		N20°29'51"W 182.84'	21+82.84	2955736.293	741122.837

CURVE TABLE				
CURVE #	DELTA	RADIUS	LENGTH	TANGENT



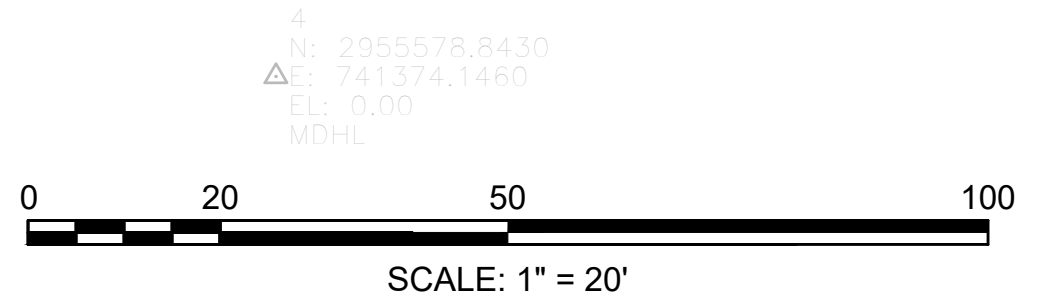
ABBREVIATIONS

BL BASELINE
 CL CENTERLINE
 STA STATION
 TRAV TRAVERSE
 TYP TYPICAL

NOTES:

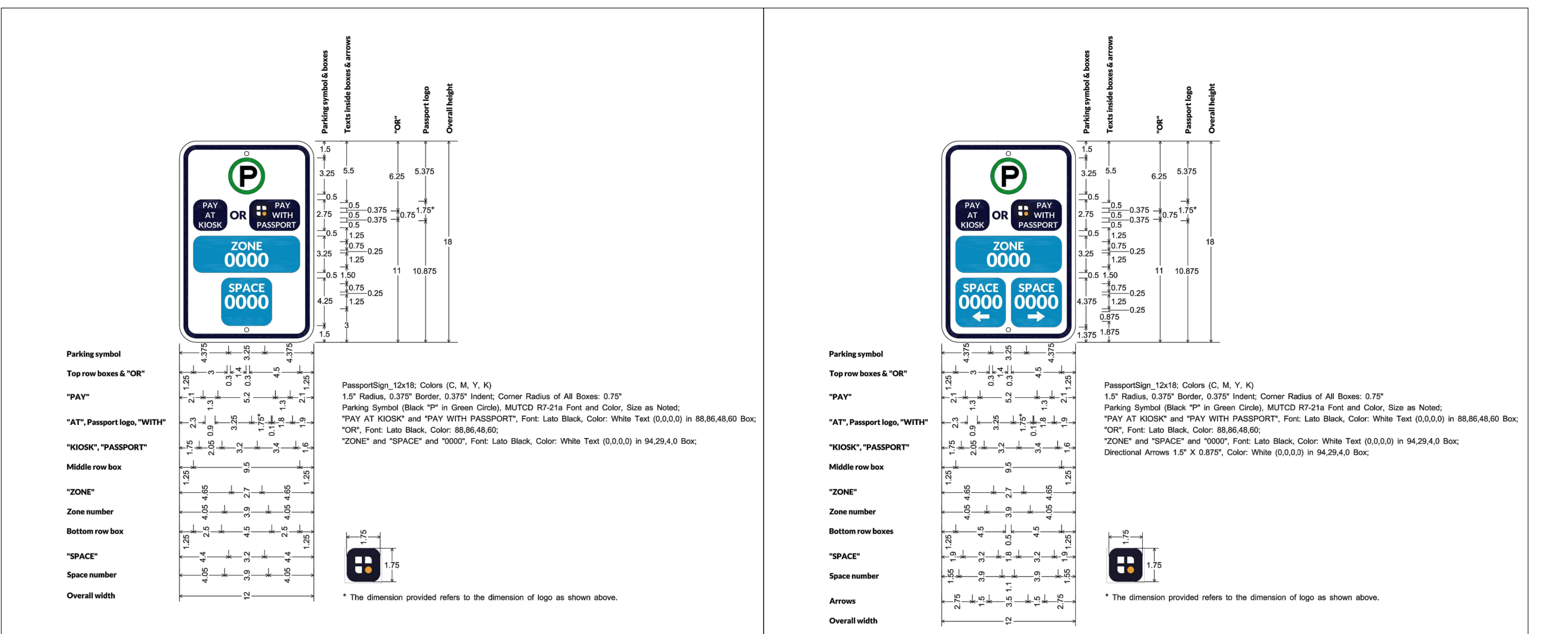
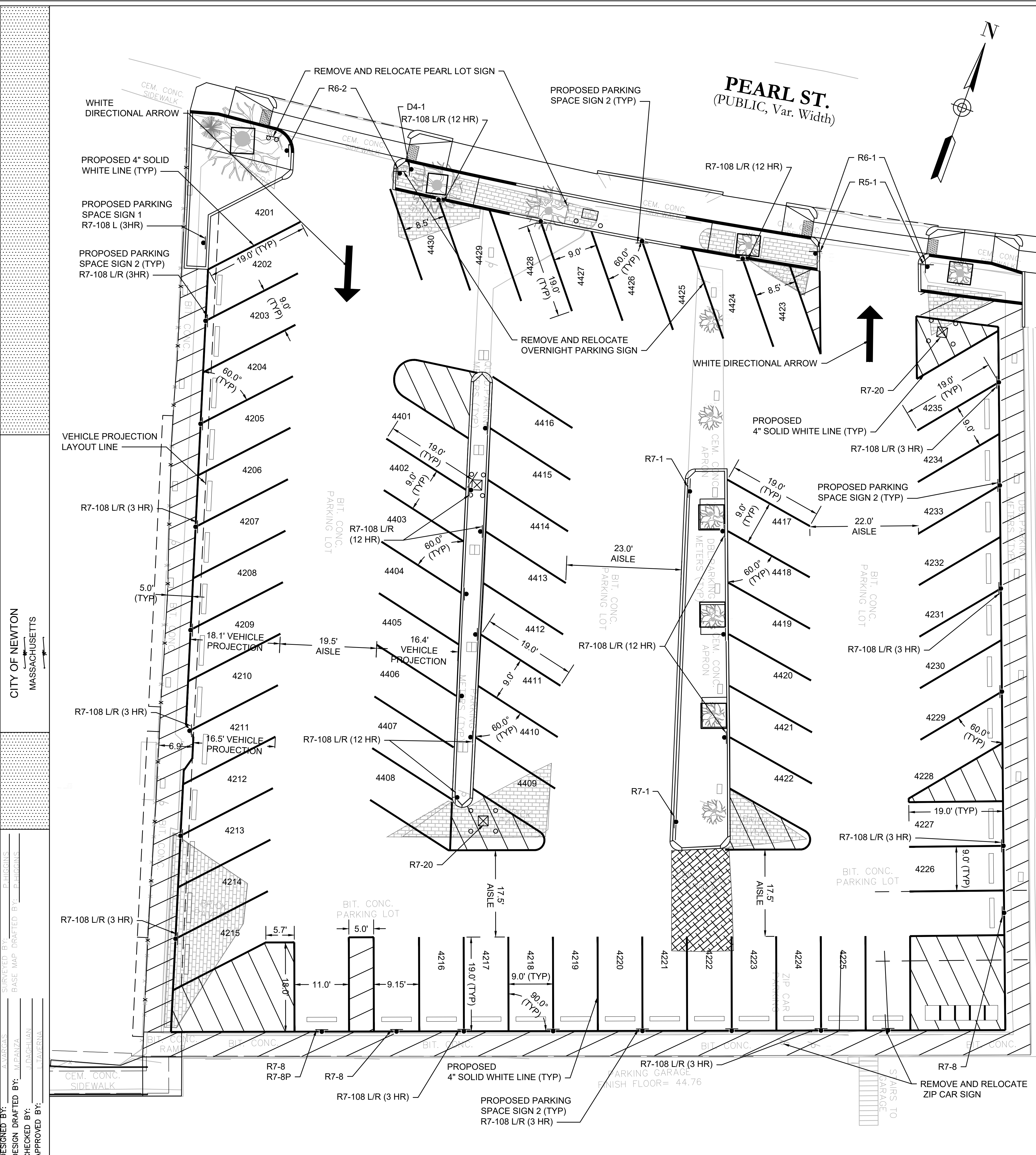
1. CONTRACTOR SHALL USE TYPICAL PARKING SPACE DIMENSIONS UNLESS OTHERWISE NOTED.

DESIGNED BY: AVZGAS
 DESIGN DRAFTED BY: M. PANZA
 CHECKED BY: M. DACHIAN
 APPROVED BY: L. LAVERNE



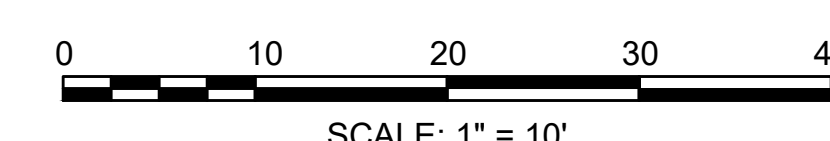
CITY OF NEWTON
 MASSACHUSETTS
 ALIGNMENT & SURVEY CONTROL PLAN
 FOR THE
 RECONSTRUCTION
 OF THE
 PEARL STREET PARKING LOT

PAGE 09 OF 16
 Louis M. Taverna, P.E. DATE: 2/28/22



IDENTIFICATION NUMBER	SIZE OF SIGN (INCHES)		TEXT	TEXT DIMENSIONS (INCHES)			NUMBER OF SIGNS REQUIRED	COLOR		
	WIDTH	HEIGHT		LETTER HEIGHT	VERTICAL SPACING	ARROW MKR. RTE.		BACKGROUND	LEGEND	BORDER
R6-1	36	12		SEE FHWA "STANDARD HIGHWAY SIGNS, 2004 EDITION"; AS AMENDED			2	BLACK	BLACK	WHITE
R6-2	24	30					2	BLACK	BLACK	WHITE
R7-1	12	18					2	WHITE	RED	RED
R7-8	12	18					3	WHITE	GREEN/WHITE SYMBOL ON BLUE	GREEN
R7-8p	18	9					1	WHITE	GREEN	GREEN
D4-1	30	24					1	GREEN	WHITE	GREEN
R5-1	30	30					2	RED	WHITE	
PARKING SPACE 1	12	18					1			
PARKING SPACE 2	12	18					32			
R7-20	24	18					2	GREEN	PAY HERE	GREEN
R7-108 L/R (3HR)	12	18					10	WHITE	GREEN	GREEN
R7-108 L/R (12HR)	12	18					8	WHITE	GREEN	GREEN
R7-108 L (3HR)	12	18					1	WHITE	GREEN	GREEN

- NOTES:
- CONTRACTOR SHALL USE TYPICAL PARKING SPACE DIMENSIONS UNLESS OTHERWISE NOTED.
 - ONE PARKING SPACE SIGN FOR TWO SPACES UNLESS OTHERWISE NOTED.
 - SPACE NUMBERS ARE FOR INFORMATIONAL PURPOSES ONLY.
 - ALL PAVEMENT MARKINGS TO BE COMPLETED IN THERMOPLASTIC.
 - R7-108 TO BE X HR 8AM-6PM EXCEPT SUNDAYS & HOLIDAYS.

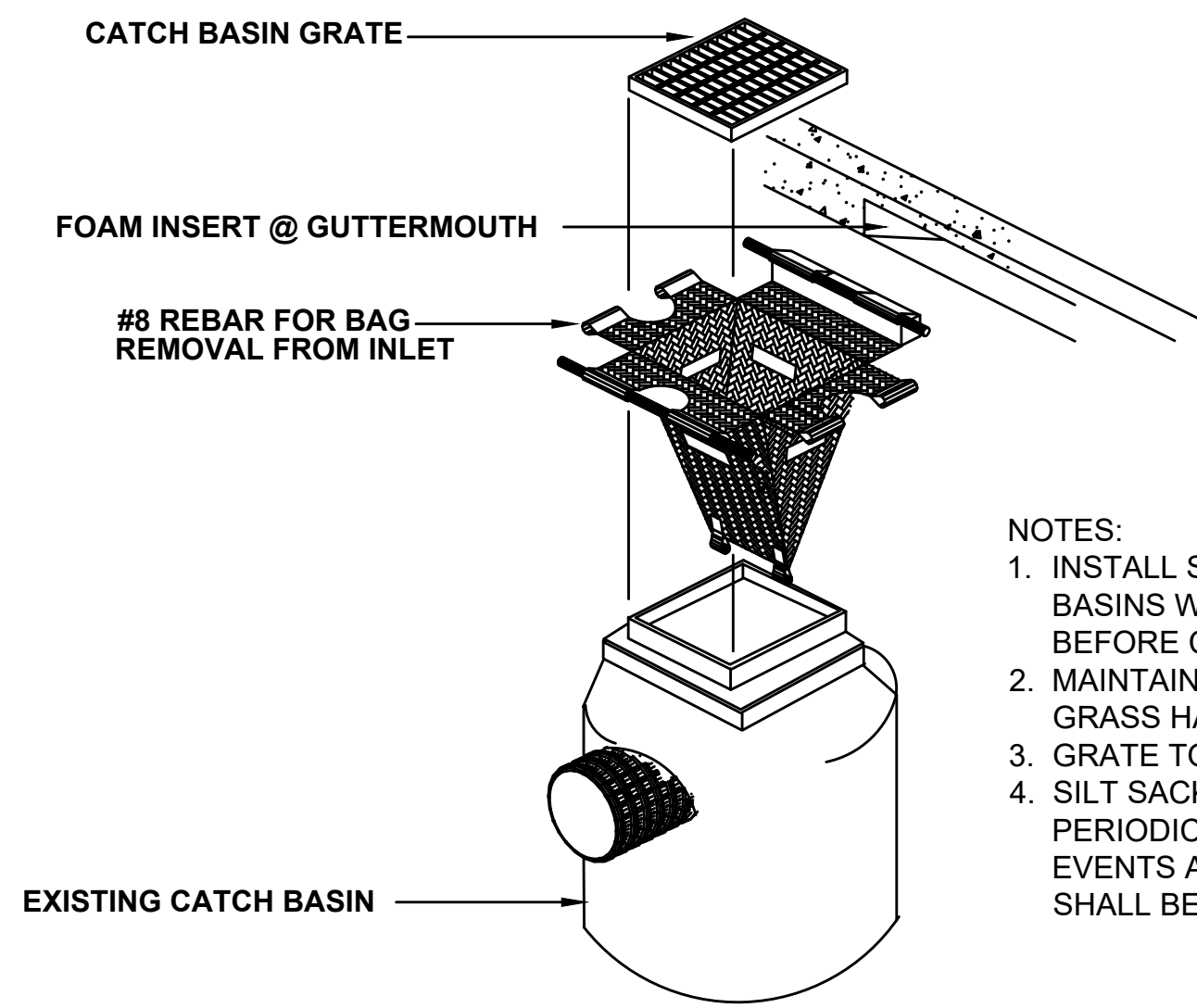


CITY OF NEWTON
 MASSACHUSETTS
 TRAFFIC PLAN
 FOR THE
 RECONSTRUCTION
 OF THE
 PEARL STREET PARKING LOT

PAGE 10 OF 16
 Louis M. Taverna, P.E. DATE: 2/28/22

SURVEYED BY: P. HIGGINS
 DESIGN DRAFTED BY: M. PANZA
 CHECKED BY: M. DASHIAN
 APPROVED BY: L. LAVERNE

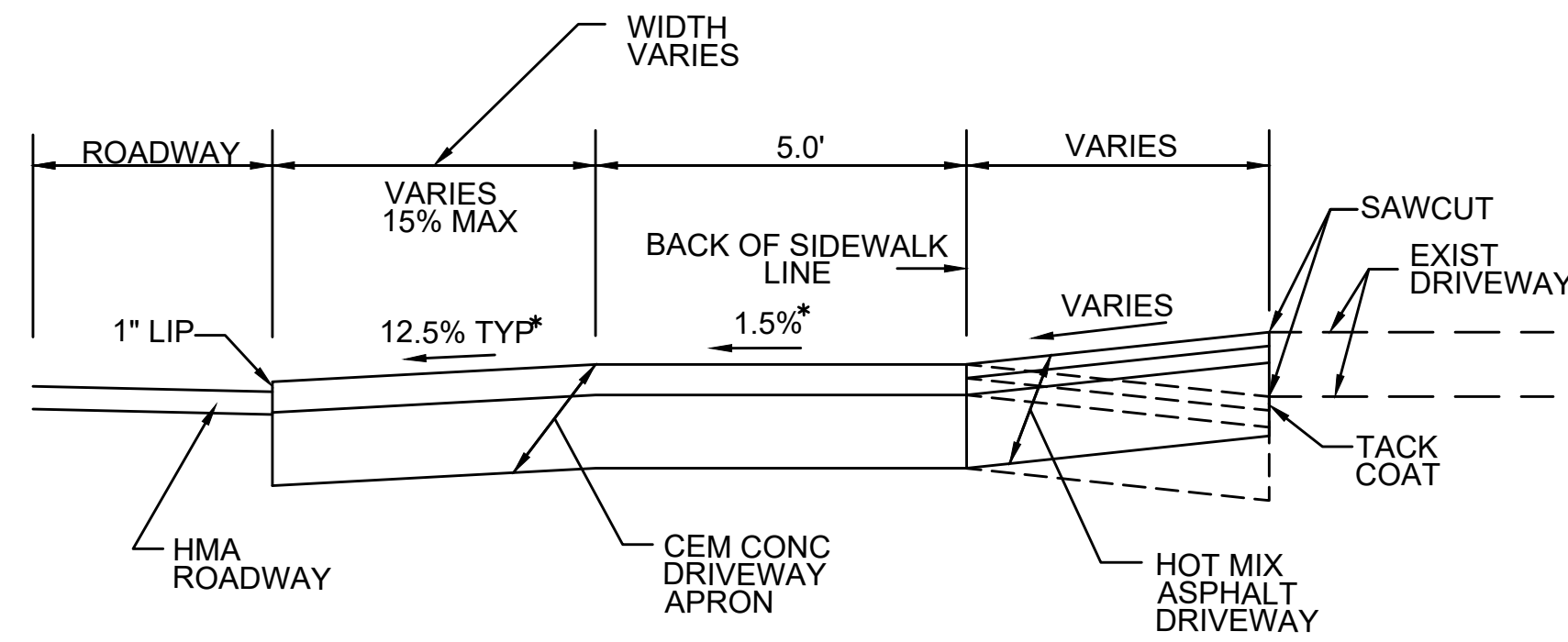
CITY OF NEWTON
 MASSACHUSETTS



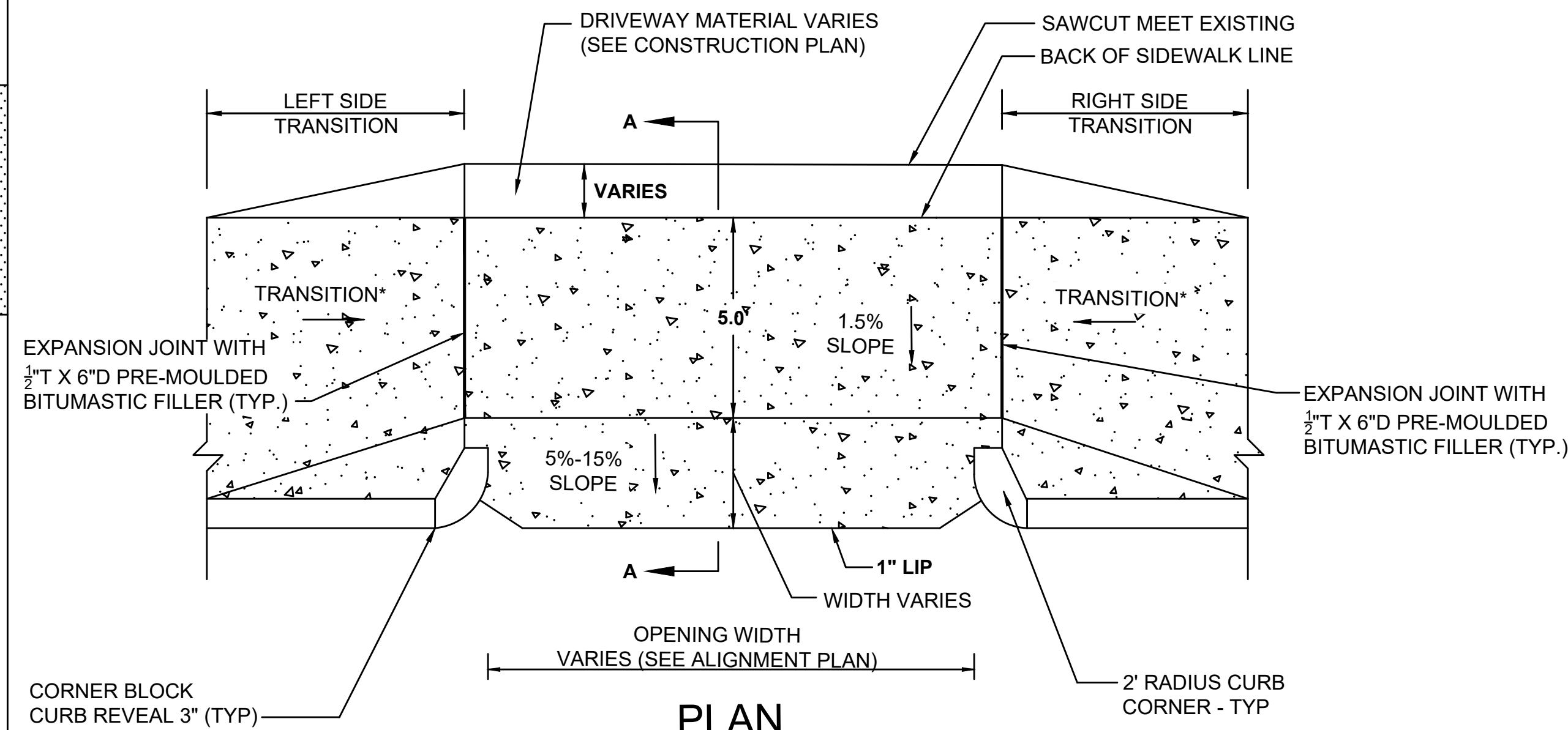
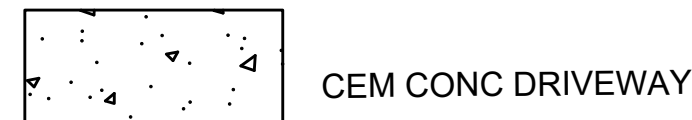
- NOTES:**
1. INSTALL SILT SACK IN EXISTING CATCH BASINS WITHIN THE LIMITS OF WORK, BEFORE COMMENCING WORK.
 2. MAINTAIN UNTIL A PERMANENT STAND OF GRASS HAS BEEN ESTABLISHED.
 3. GRATE TO BE PLACED OVER SILT SACK.
 4. SILT SACK SHALL BE INSPECTED PERIODICALLY AND AFTER ALL STORM EVENTS AND CLEANING OR REPLACEMENT SHALL BE PERFORMED

TYPICAL 2'X2' SILT SACK

SCALE: N.T.S.



SECTION A-A

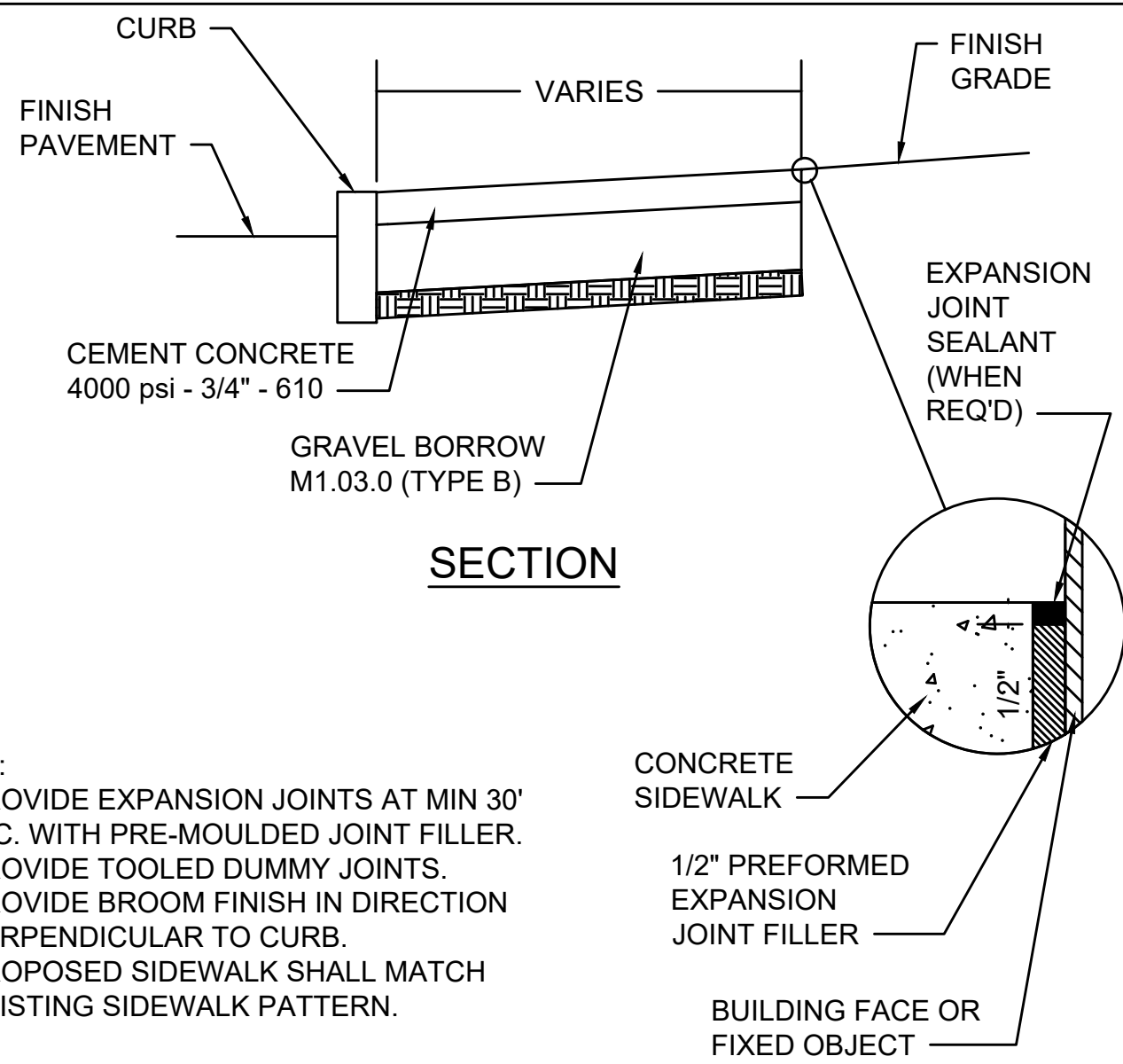


* TOLERANCE FOR CONSTRUCTION ±0.5%

CEMENT CONCRETE DRIVEWAY APRON

SCALE: N.T.S.

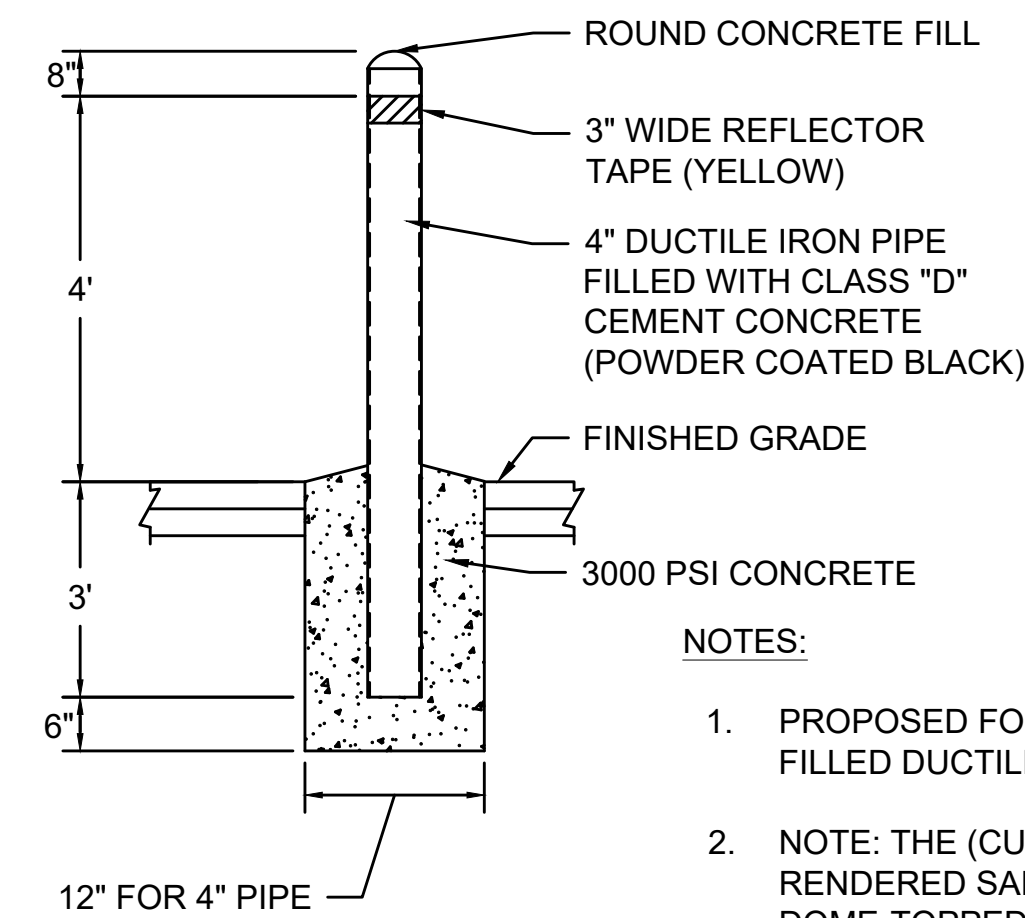
NOTE: CURB SHALL BE SET IN CONCRETE.



- NOTES:**
1. PROVIDE EXPANSION JOINTS AT MIN 30' O.C. WITH PRE-MOULDED JOINT FILLER.
 2. PROVIDE TOOLED DUMMY JOINTS.
 3. PROVIDE BROOM FINISH IN DIRECTION PERPENDICULAR TO CURB.
 4. PROPOSED SIDEWALK SHALL MATCH EXISTING SIDEWALK PATTERN.

CEMENT CONCRETE SIDEWALK

SCALE: N.T.S.

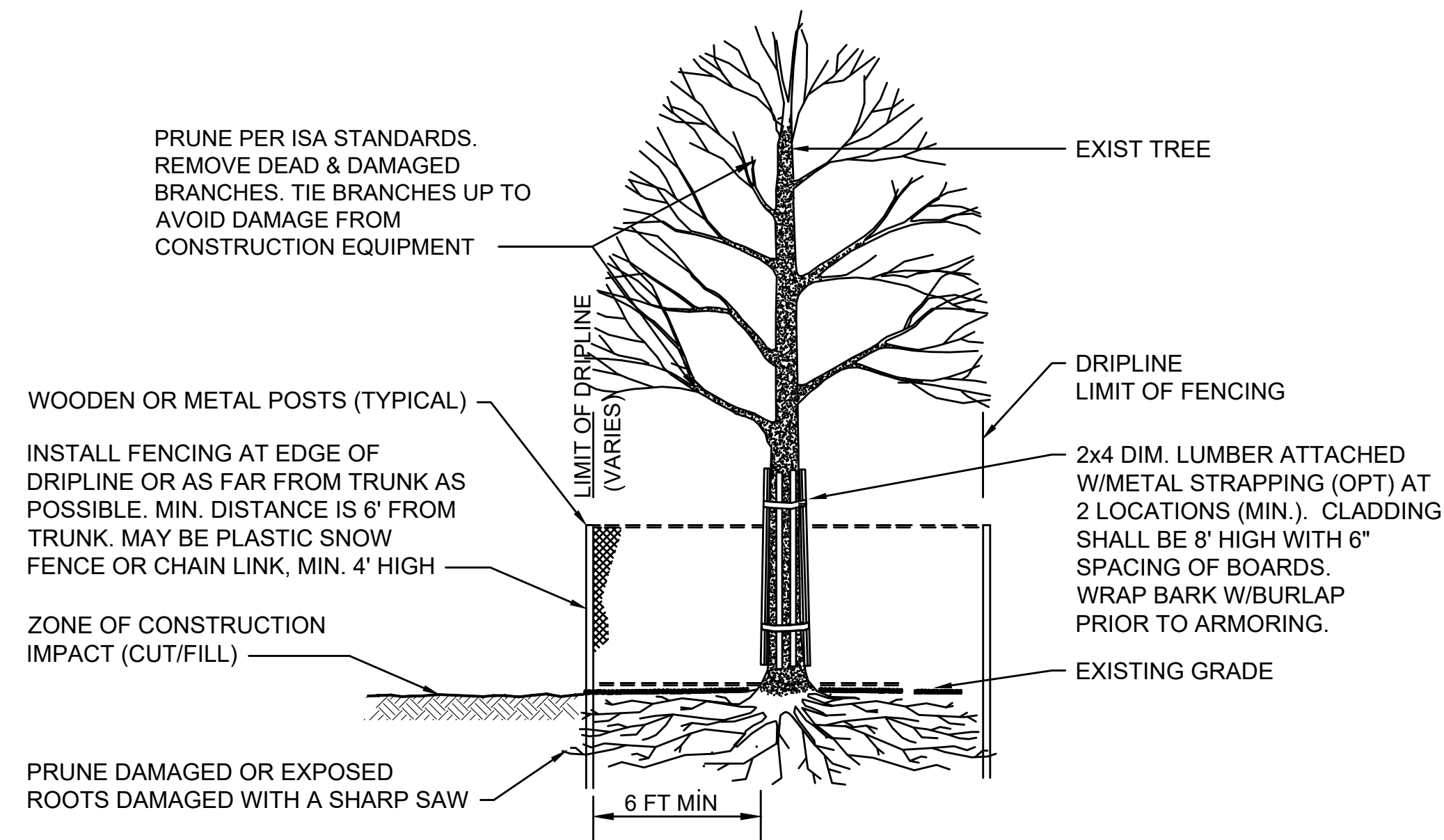


- NOTES:**
1. PROPOSED FOUR INCH (4") DIAMETER CONCRETE FILLED DUCTILE IRON FIELD FABRICATED BOLLARD.
 2. NOTE: THE (CUT) EDGES ARE TO BE BEVELED & RENDERED SAFE PRIOR TO PLACEMENT OF THE DOME-TOPPED CONCRETE FILLER MATERIAL.

BOLLARD

SCALE: N.T.S.

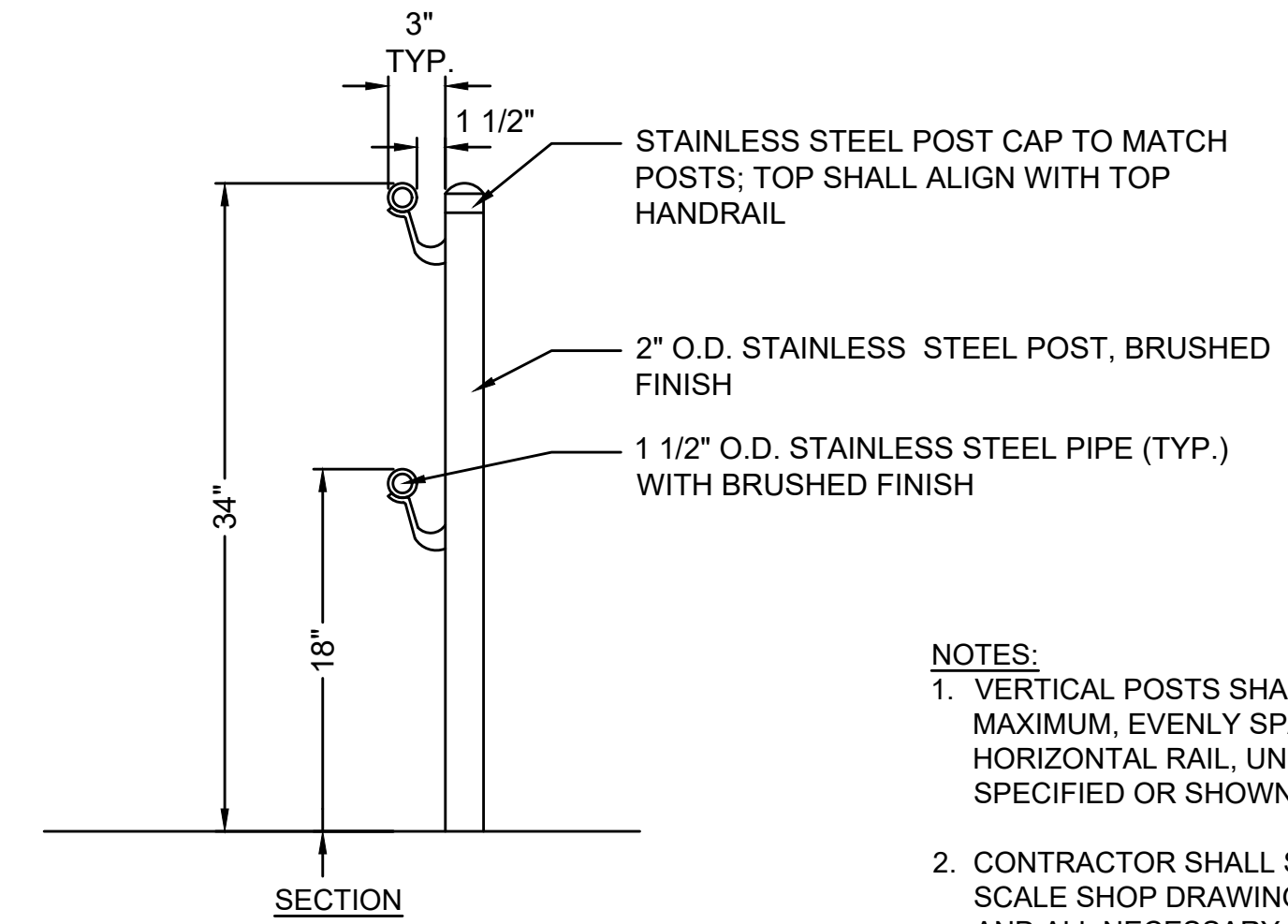
PRUNE PER ISA STANDARDS. REMOVE DEAD & DAMAGED BRANCHES. TIE BRANCHES UP TO AVOID DAMAGE FROM CONSTRUCTION EQUIPMENT



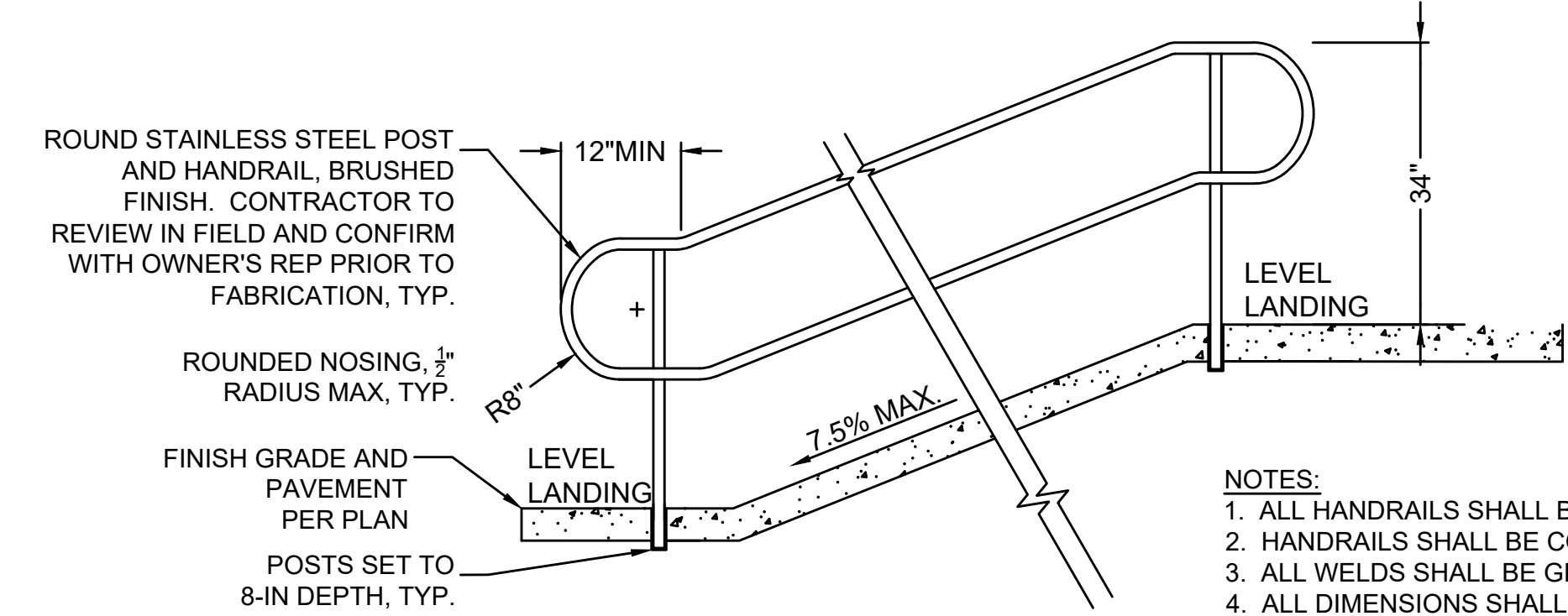
- NOTES:**
1. CRITICAL ROOT ZONE TO BE PROTECTED. ALL WORK NECESSARY WITHIN THE CRITICAL ROOT ZONE SHALL BE PERFORMED BY HAND AND LIGHT EQUIPMENT, OR AS DIRECTED BY THE ENGINEER.
 2. ROOTS EXPOSED DURING EXCAVATION SHALL BE NEATLY CUT AND COVERED WITH SOIL IMMEDIATELY.
 3. FOR TREES THAT OCCUR IN GROUPS PROVIDE TREE PROTECTION FENCE AROUND ENTIRE AREA.
 4. MAINTAIN FENCE PROTECTION IN SOUND CONDITION UNTIL PROJECT COMPLETION.
 5. A CERTIFIED ARBORIST SHALL DELINEATE LIMIT OF TREE PROTECTION FENCE AS THEY RELATE TO THE LIMITS OF THE CRITICAL ROOT ZONE.
 6. NO STORAGE OF EQUIPMENT OR STOCKPILING OF MATERIALS WITHIN DRIPLINE.

TREE PROTECTION

SCALE: N.T.S.



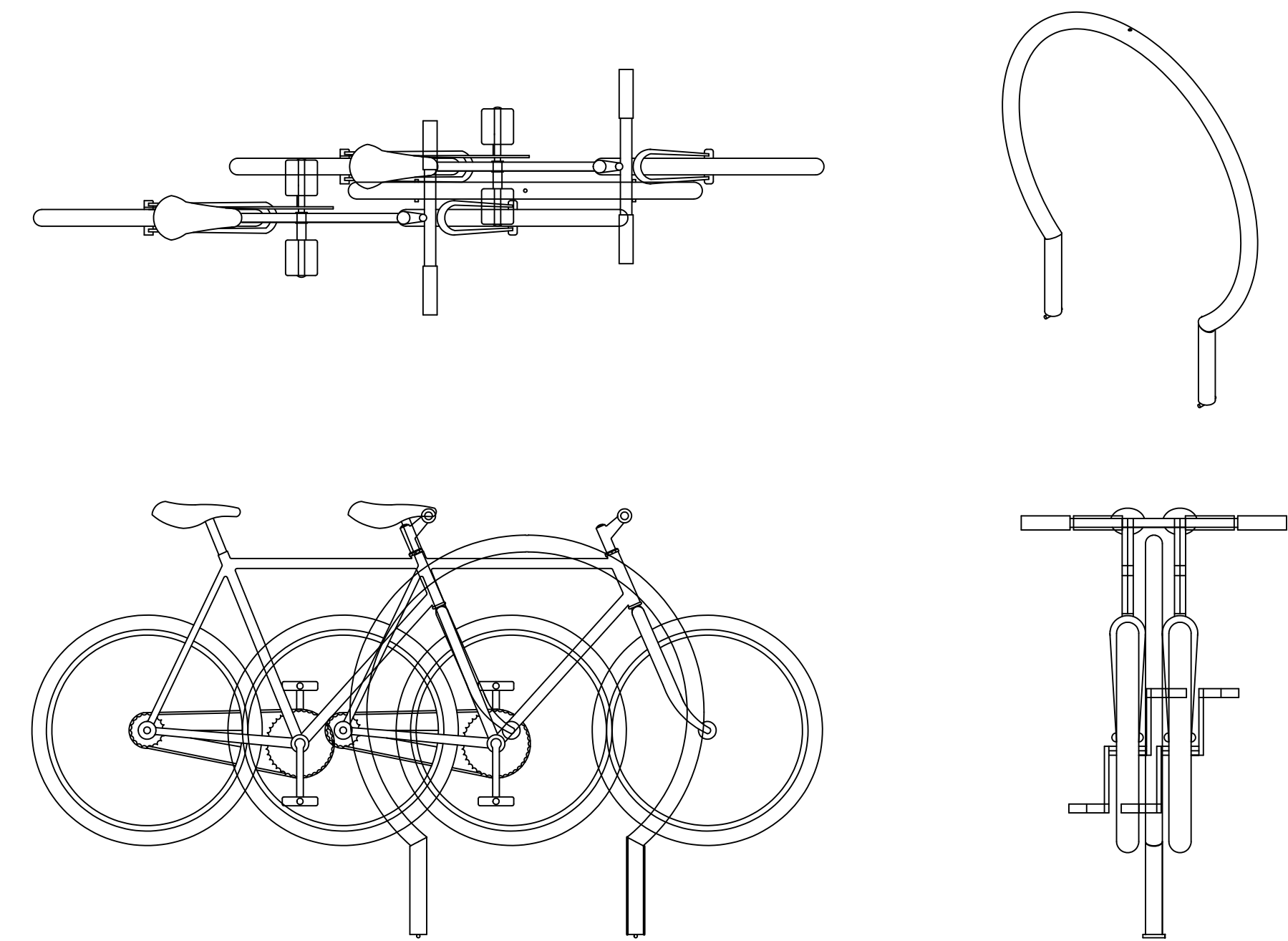
- NOTES:**
1. VERTICAL POSTS SHALL BE SET @ 6' MAXIMUM, EVENLY SPACED ALONG THE HORIZONTAL RAIL, UNLESS OTHERWISE SPECIFIED OR SHOWN ON THE DRAWINGS.
 2. CONTRACTOR SHALL SUBMIT LARGE SCALE SHOP DRAWINGS OF HANDRAILS AND ALL NECESSARY HARDWARE PER SPECIFICATIONS



- NOTES:**
1. ALL HANDRAILS SHALL BE STAINLESS STEEL.
 2. HANDRAILS SHALL BE CORE DRILLED AND SET WITH EPOXY GROUT.
 3. ALL WELDS SHALL BE GROUND DOWN CLEAN AND SMOOTH.
 4. ALL DIMENSIONS SHALL BE FIELD VERIFIED AND ADJUSTED AS NEEDED.

HAND RAIL

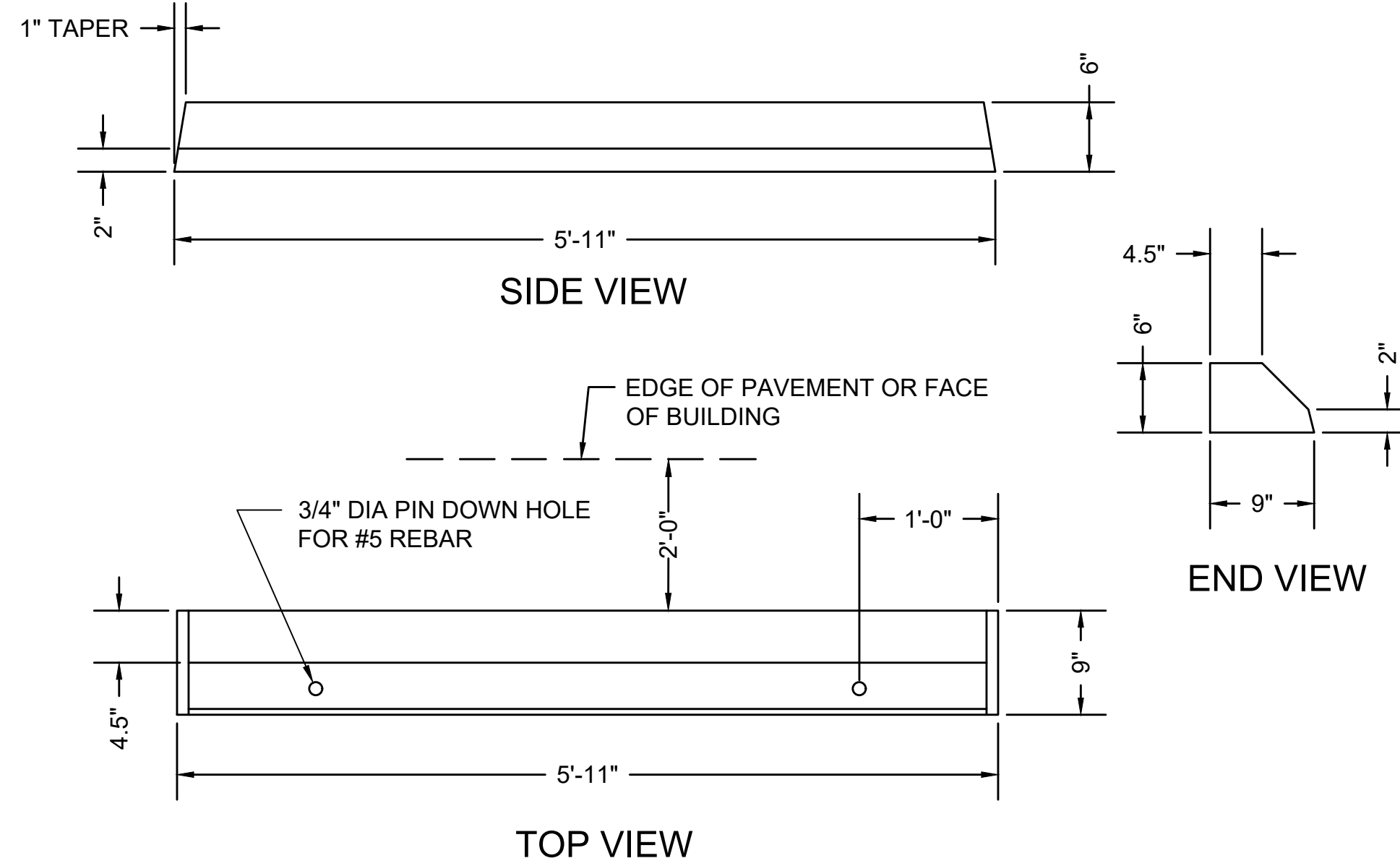
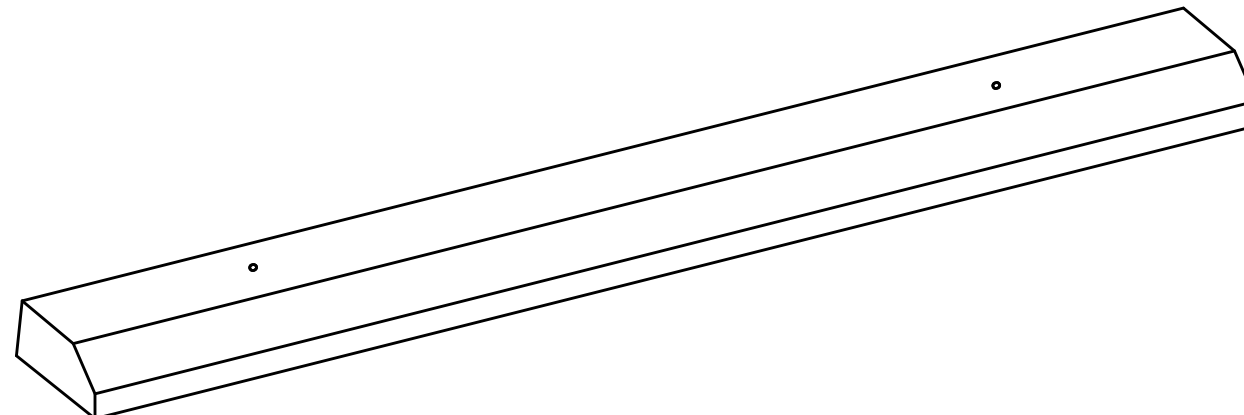
SCALE: N.T.S.



BIKE RACK - ROUND, INGROUND MOUNT

SCALE: N.T.S.

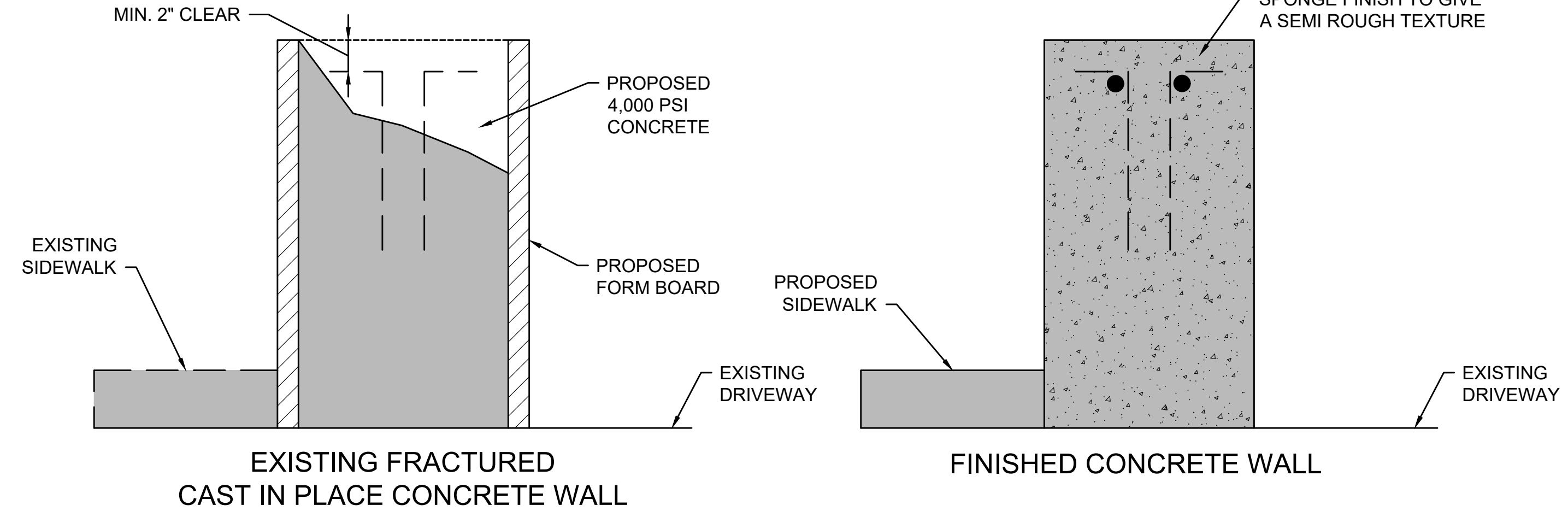
CITY OF NEWTON
MASSACHUSETTS
CONSTRUCTION DETAILS
FOR THE
RECONSTRUCTION
OF THE
PEARL STREET PARKING LOT



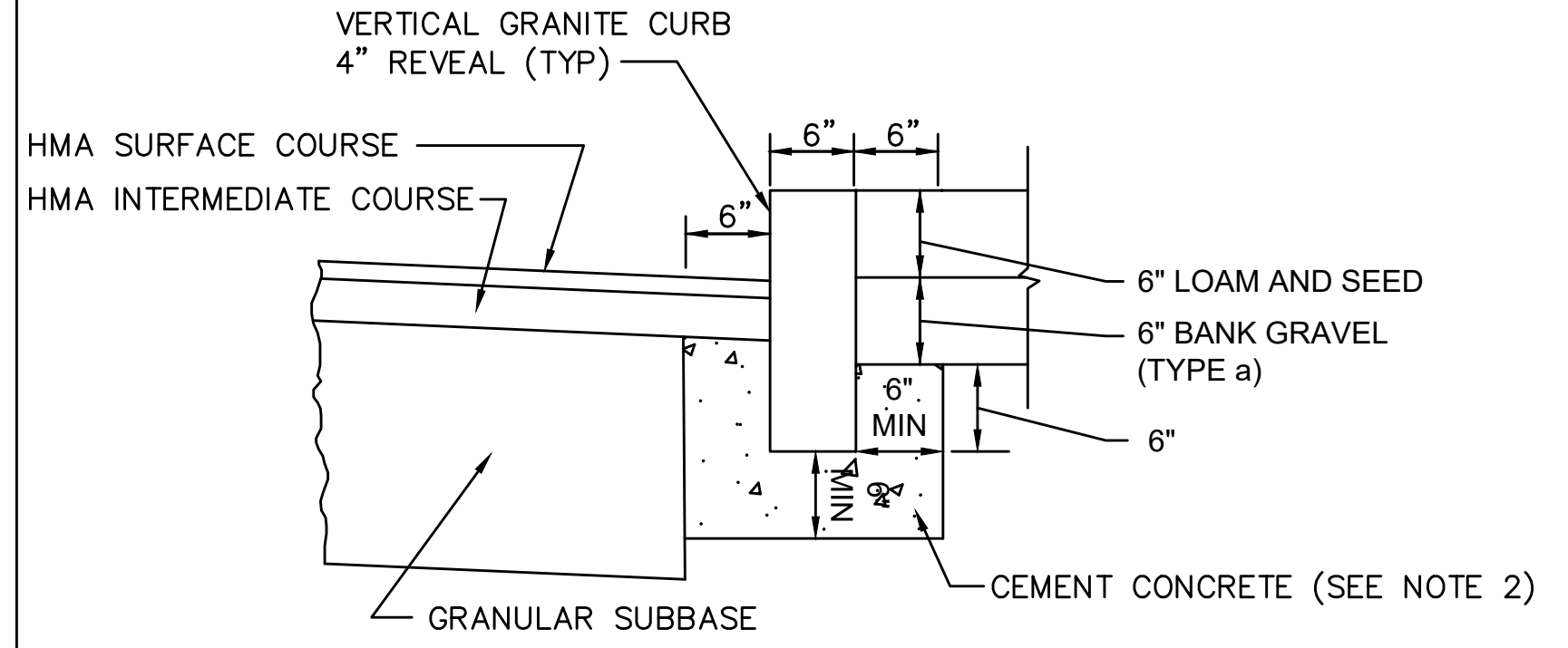
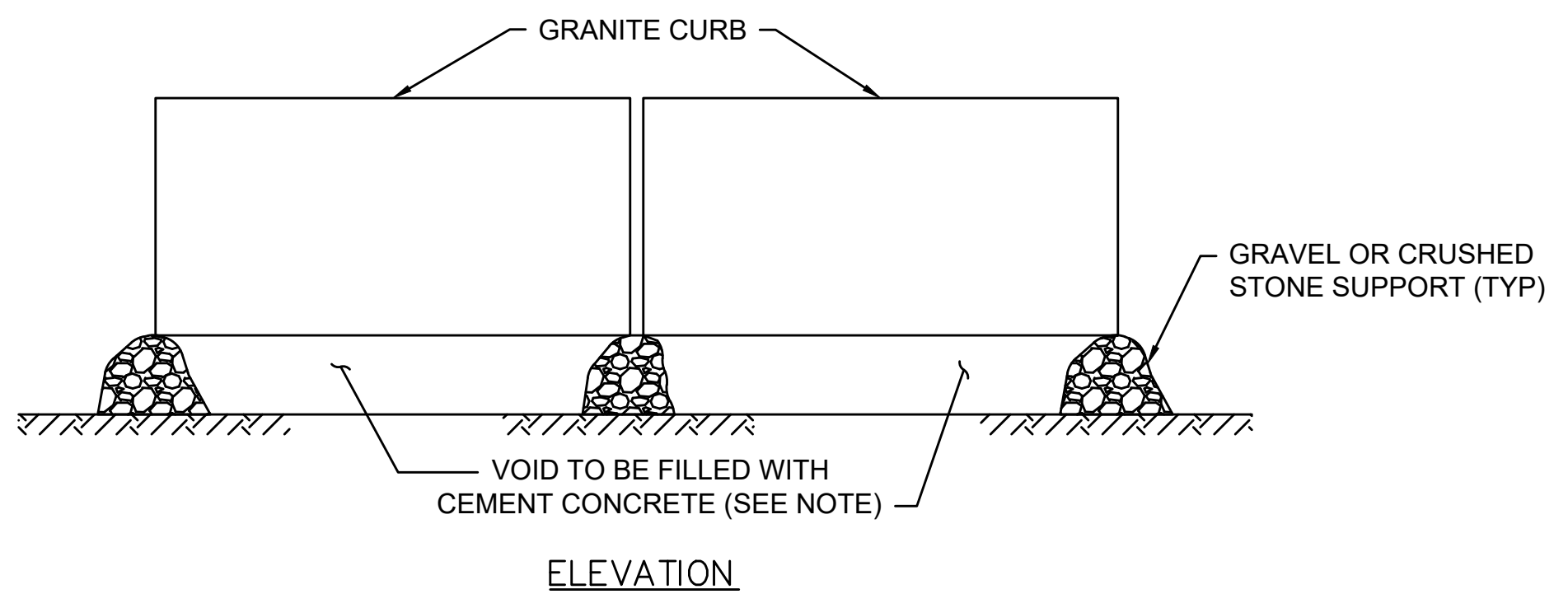
- NOTES:**
1. CONCRETE: 4,000 PSI MINIMUM AFTER 28 DAYS.
 2. PROVIDE 2 #5 REBAR STEEL PINS PER WHEEL STOP.
 3. #5 REBAR STEEL PINS SHALL EXTEND DOWN 2' MIN. FROM THE FINISHED GRADE.

CONCRETE WHEEL STOP
SCALE: N.T.S.

- NOTES:**
1. SURFACE OF CONCRETE WALL MUST BE CLEANED OF ALL LOOSE MATERIAL.
 2. #3 VERTICAL REBAR SHALL BE DRILLED AND EMBEDDED A MINIMUM OF 6" WITH AN EPOXY SETTING COMPOUND AT 12" ON CENTER.
 3. #3 HORIZONTAL REBAR SHALL BE TIED TO VERTICAL BARS AND MUST HAVE A MINIMUM CLEARANCE FROM THE TOP OF 2" CLEAR.
 4. SEE ITEM 908.5 SPECIFICATION FOR ADDITIONAL INFORMATION.

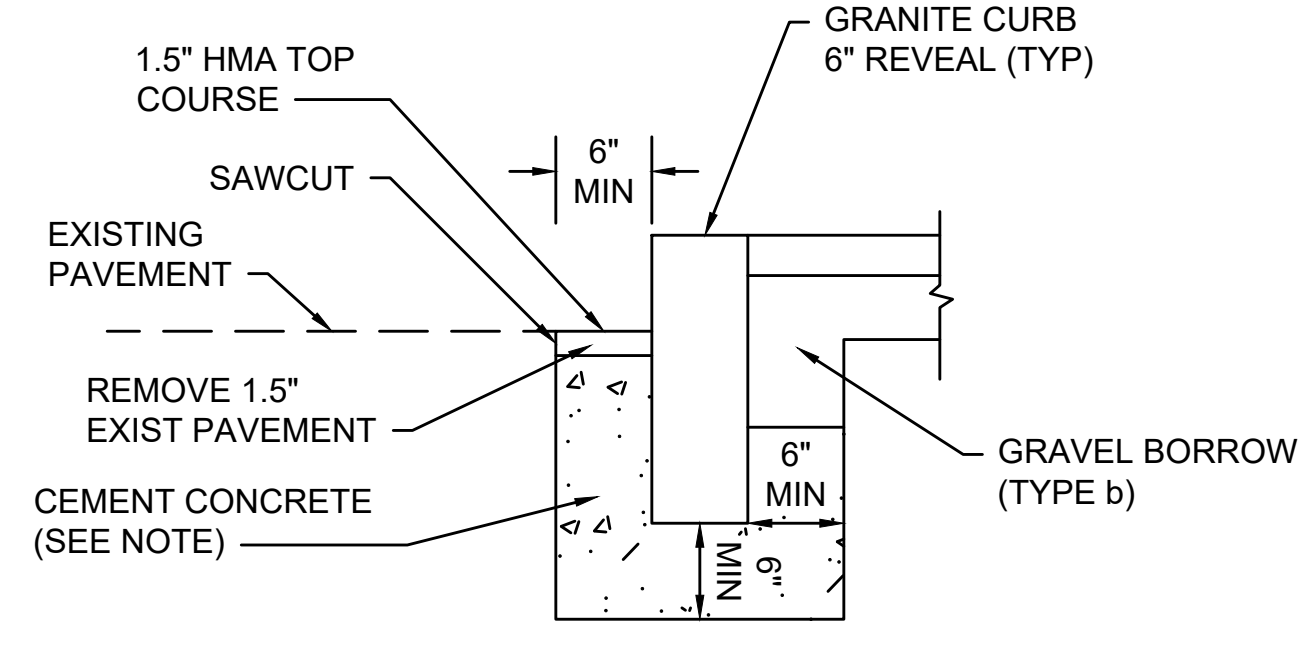


RETAINING WALL REPAIR DETAIL
SCALE: N.T.S.



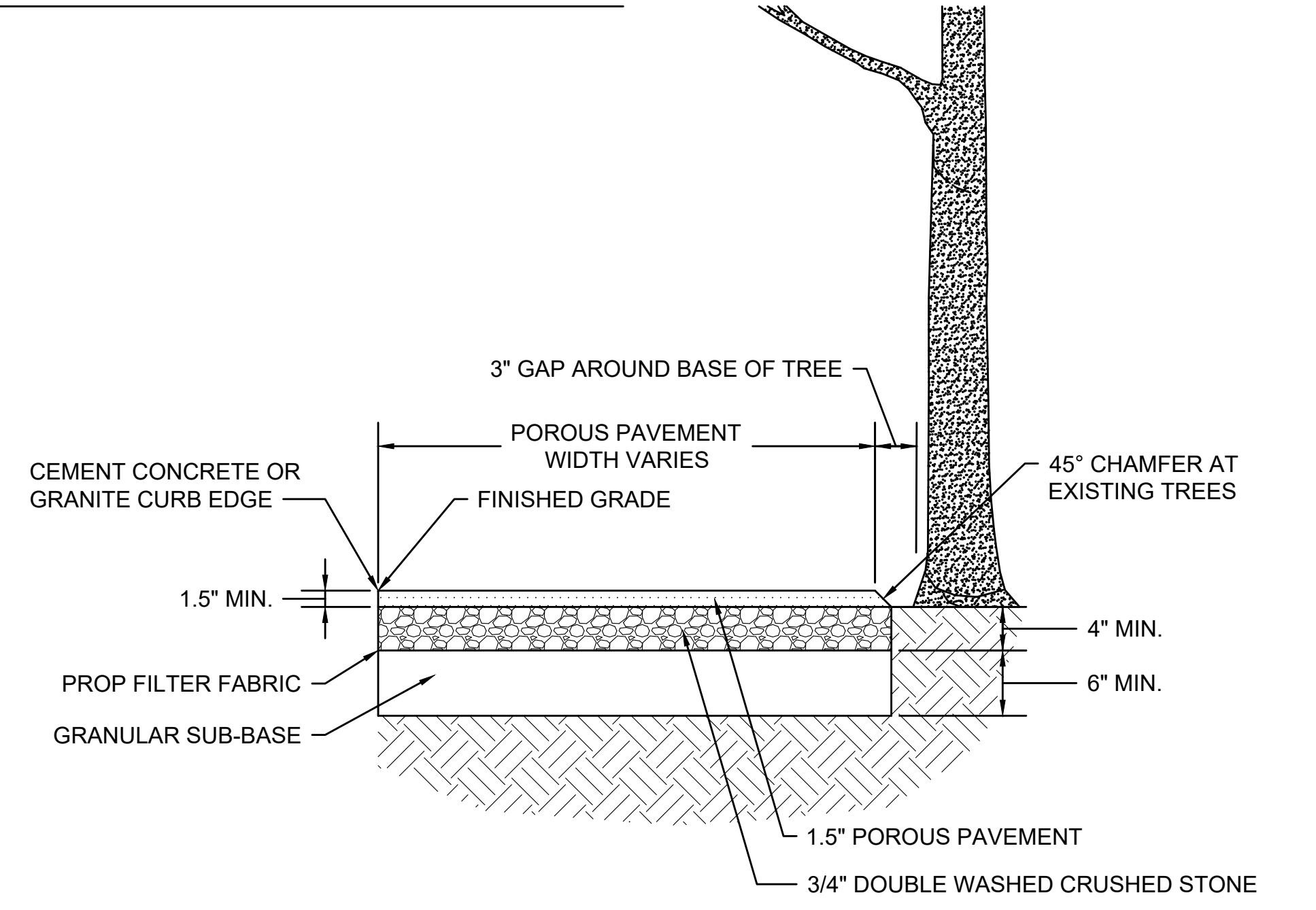
- NOTES:**
1. THIS PROCEDURE IS APPLICABLE ONLY IF CURB IS TO BE SET AFTER SUBBASE COURSE IS IN PLACE PRIOR TO INTERMEDIATE AND SURFACE PLACEMENT.
 2. ANY DESIGNATED CEMENT CONCRETE THAT IS ACCEPTABLE UNDER SECTION M4 OF THE STANDARD SPECIFICATIONS MAY BE USED. ALL TEST REQUIREMENTS ARE WAIVED. HOT MIX ASPHALT SHALL NOT BE USED AS A SUBSTITUTE.

GRANITE CURB IN FULL DEPTH RECLAMATION
SCALE: N.T.S.

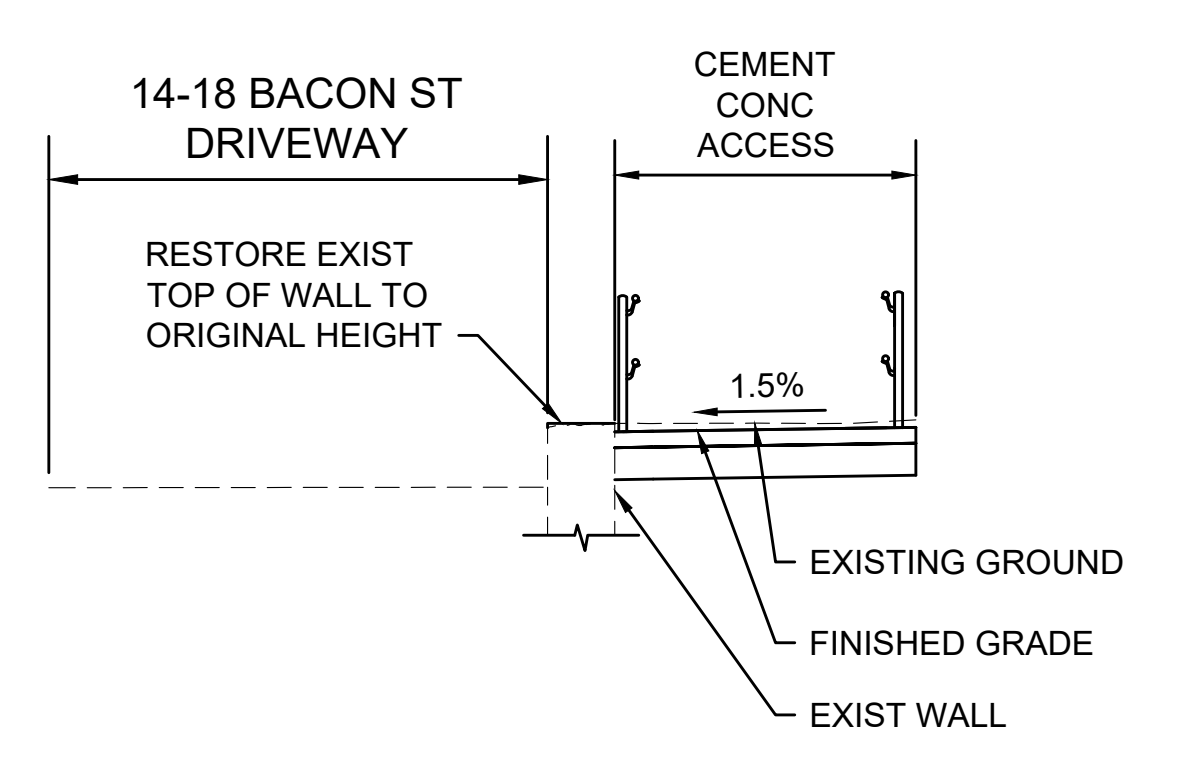
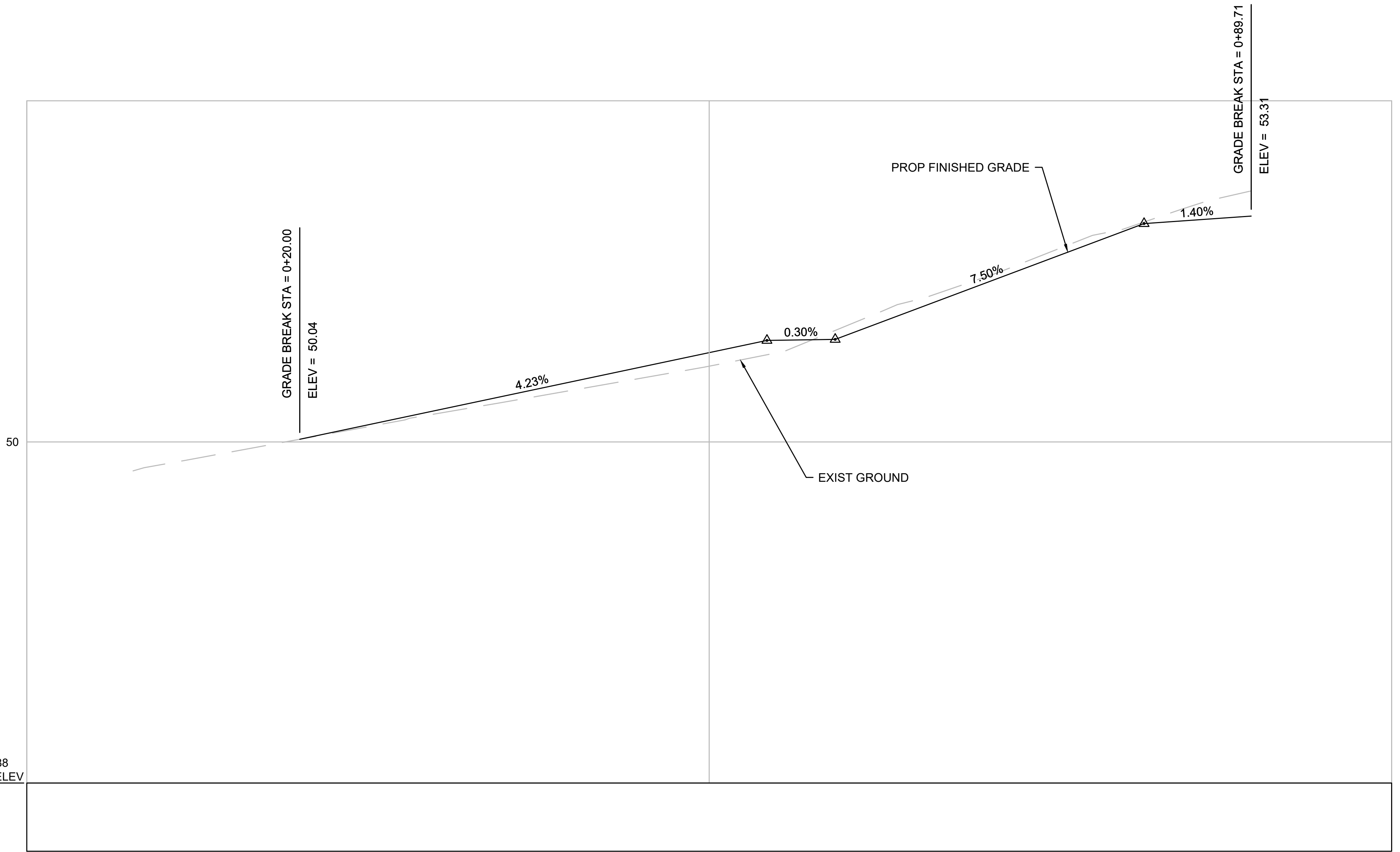
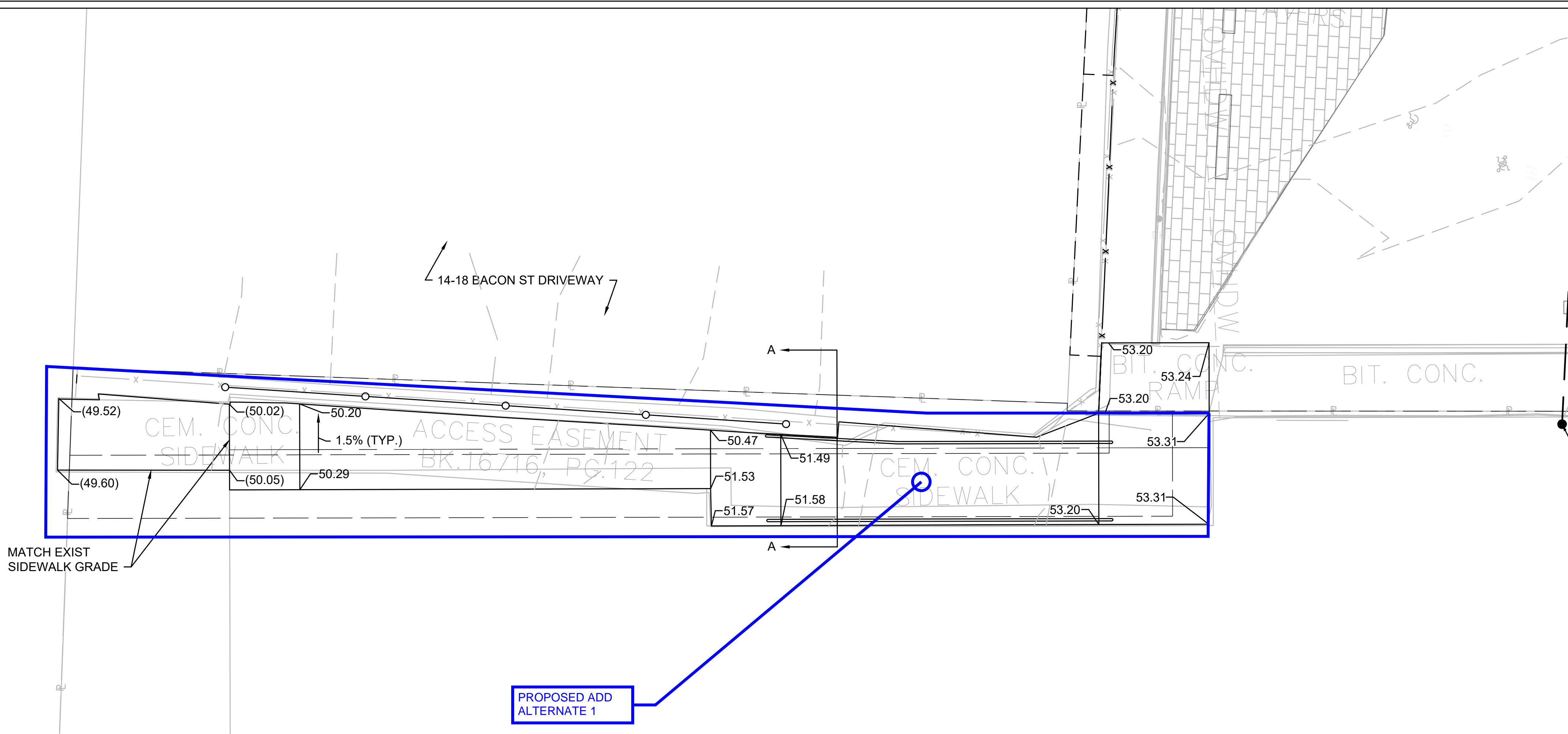


NOTE: ANY DESIGNATED CEMENT CONCRETE THAT IS ACCEPTABLE UNDER SECTION M4 OF THE STANDARD SPECIFICATIONS MAY BE USED. ALL TEST REQUIREMENTS ARE WAIVED. HOT MIX ASPHALT SHALL NOT BE USED AS A SUBSTITUTE.

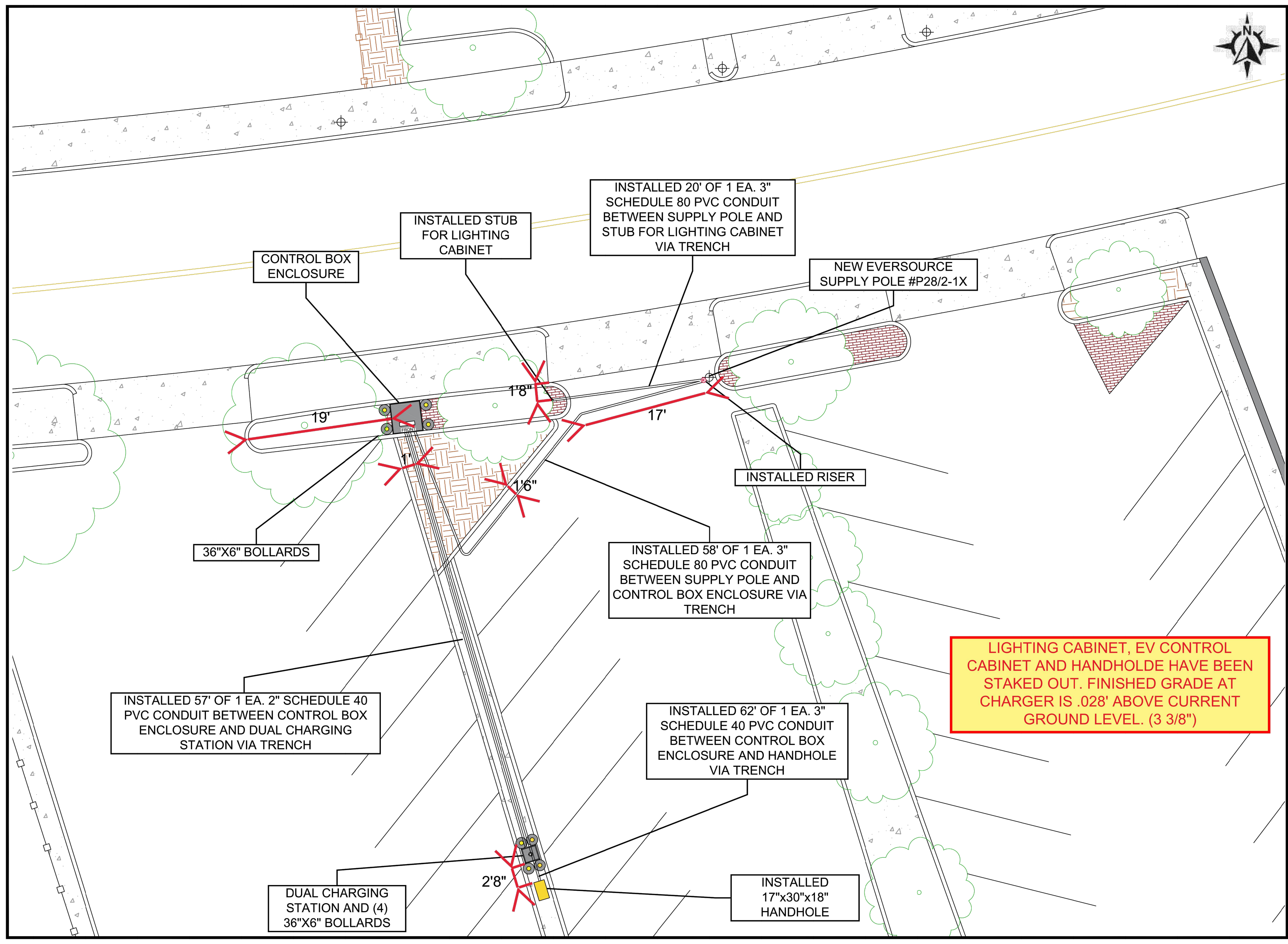
GRANITE CURB IN EXISTING PAVEMENT
SCALE: N.T.S.



POROUS PAVEMENT DETAIL
SCALE: N.T.S.



TYPICAL SECTION A-A
BACON ST ACCESS



MAVERICK

1 Westinghouse Plaza,
Suite D6, Boston,
Massachusetts, 02136
+1 617-361-6700

EVERSOURCE ENERGY

800 Boylston St,
Boston,
Massachusetts, 02199
+1 800-592-2000

PROJECT NO: 10-19-0005.361
 DRAWN BY: D.D.
 CHECKED BY: D.C.

REV	DATE	DESCRIPTION
0	02/25/2022	AS-BUILT
3	05/29/2021	ISSUED FOR REVIEW
2	03/11/2021	ISSUED FOR REVIEW
1	02/03/2021	ISSUED FOR REVIEW
0	06/29/2020	ISSUED FOR REVIEW

IT IS A VIOLATION OF THE LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, TO ALTER THIS DOCUMENT

CITY OF NEWTON, PEARL STREET PARKING LOT, 16-18 PEARL ST, NEWTON, MA 02458, USA

ELECTRIC VEHICLE CHARGING STATIONS

SHEET TITLE
SITE PLAN

SHEET NUMBER
C-4




- NOTES:
1. THE CONTENT OF THIS SHEET REFERS TO WORK TO BE DONE BY OTHERS PRIOR TO OR IN CONJUNCTION WITH THIS PROJECT AND IS SHOWN FOR INFORMATION PURPOSES ONLY.
 2. IF ANY OF THE WORK SHOWN ON THIS SHEET OCCURS DURING THE COURSE OF THE PEARL LOT RECONSTRUCTION, THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR ACCESS AND INSTALLATION OF THE WORK SHOWN ON THIS SHEET BY THE UTILITY COMPANY.

CITY OF NEWTON
MASSACHUSETTS
EV CHARGERS
FOR THE
RECONSTRUCTION
OF THE
PEARL STREET PARKING LOT

GENERAL NOTES - PHASE 1, 2, 3 AND 3A

1. ALL TEMPORARY TRAFFIC CONTROL WORK SHALL CONFORM TO THE LATEST EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) AND ALL REVISIONS AS WELL AS THE MASSDOT STANDARD DETAILS AND DRAWINGS FOR THE DEVELOPMENT ON TRAFFIC MANAGEMENT PLANS.
2. AUTHORIZATION OF THE NEWTON POLICE DEPARTMENT IS REQUIRED FOR ALL TEMPORARY PARKING RESTRICTIONS AND ROAD CLOSURES.
3. MAINTAIN ABUTTER ACCESS AT ALL TIMES, EXCEPT FOR VERY SHORT PERIODS APPROVED BY THE ENGINEER. CONTRACTORS SHALL NOTIFY EACH ABUTTER AT LEAST 24-HOURS IN ADVANCE OF THE START OF ANY WORK THAT WILL REQUIRE CLOSURE OF ACCESS.
4. ALL CONSTRUCTION SIGNS SHALL CONFORM TO THE MUTCD
5. ALL SIGNS, INCLUDING EXISTING, THAT ARE NOT REPRESENTATIVE OF ACTUAL WORK CONDITIONS SHALL BE EITHER COVERED OR REMOVED WHEN NOT APPLICABLE.
6. EXISTING PAVEMENT MARKINGS WHICH ARE IN CONFLICT WITH TEMPORARY TRAFFIC CONTROLS SHALL BE COVERED TEMPORARILY WITH BLACKOUT PAINT OR TAPE, AS REQUIRED BY THE ENGINEER.
7. TEMPORARY CONSTRUCTION SIGNING AND ALL OTHER TRAFFIC CONTROL DEVICES SHALL BE IN PLACE PRIOR TO THE START OF ANY WORK.
8. WORK ZONE SNOW REMOVAL IS INCLUDED IN THE RESPONSIBILITY OF THE CONTRACTOR.
9. A PROTECTED PEDESTRIAN PATH TO BEACON ST THROUGH THE ALLEY SHALL BE MAINTAINED DURING PHASES 2, 3 AND 3A WITH SIGNS AND CHANNELIZING DEVICES IN CONFORMANCE WITH THE MUTCD.
10. A PROTECTED PEDESTRIAN PATH TO PEARL ST SHALL BE MAINTAINED DURING ALL PHASES OF CONSTRUCTION WITH SIGNS AND CHANNELIZING DEVICES IN CONFORMANCE WITH THE MUTCD.

LEGEND:

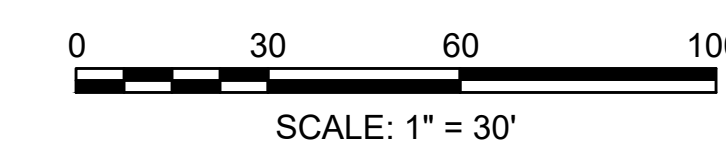
-  DIRECTION OF TRAFFIC
-  WORK ZONE
-  TEMPORARY PAVEMENT MARKINGS

PHASE 1
 MAINTAIN CENTER AND EAST ISLE OPEN DURING PHASE 1

PHASE 3
 MAINTAIN ALLEY, WEST, EAST AND SOUTH ISLE OPEN DURING PHASE 3

PHASE 2
 MAINTAIN ALLEY, WEST AND CENTER ISLE OPEN DURING PHASE 2

PHASE 3A
 MAINTAIN ALLEY, WEST, CENTER AND EAST OPEN ISLE DURING PHASE 3A



CITY OF NEWTON
 MASSACHUSETTS
 CONSTRUCTION PHASING PLAN
 FOR THE
 RECONSTRUCTION
 OF THE
 PEARL STREET PARKING LOT

CITY OF NEWTON
 MASSACHUSETTS

DESIGNED BY: A. VIZCARRA
 SURVEYED BY: P. HIGGINS
 CHECKED BY: M. PANZA
 BASE MAP DRAFTED BY: P. HIGGINS
 DRAFTED BY: M. PANZA
 APPROVED BY: L. LAVERNE