

1314 WASHINGTON STREET

WEST NEWTON, MA



GROUND FLOOR LEVEL

ORIGINAL

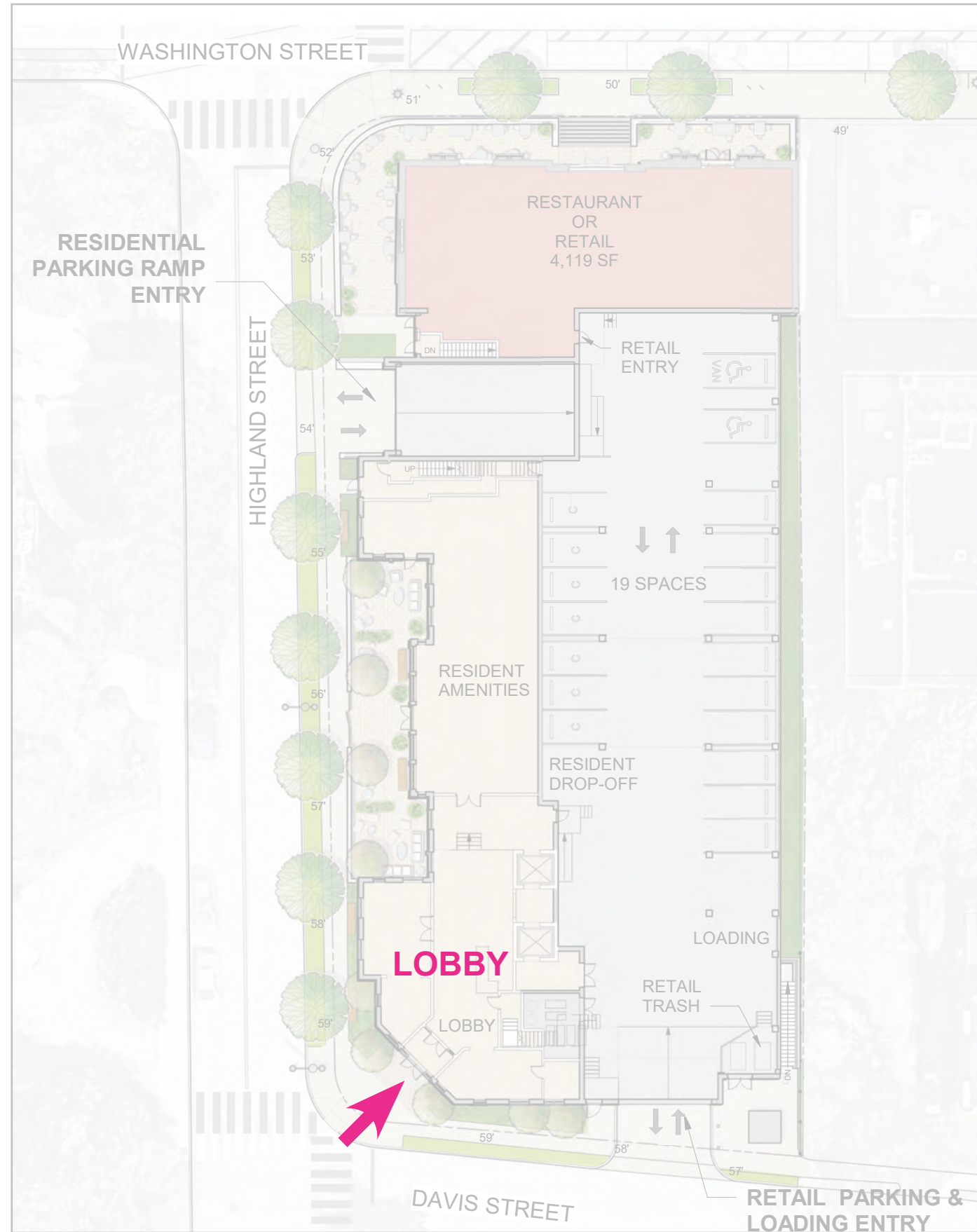


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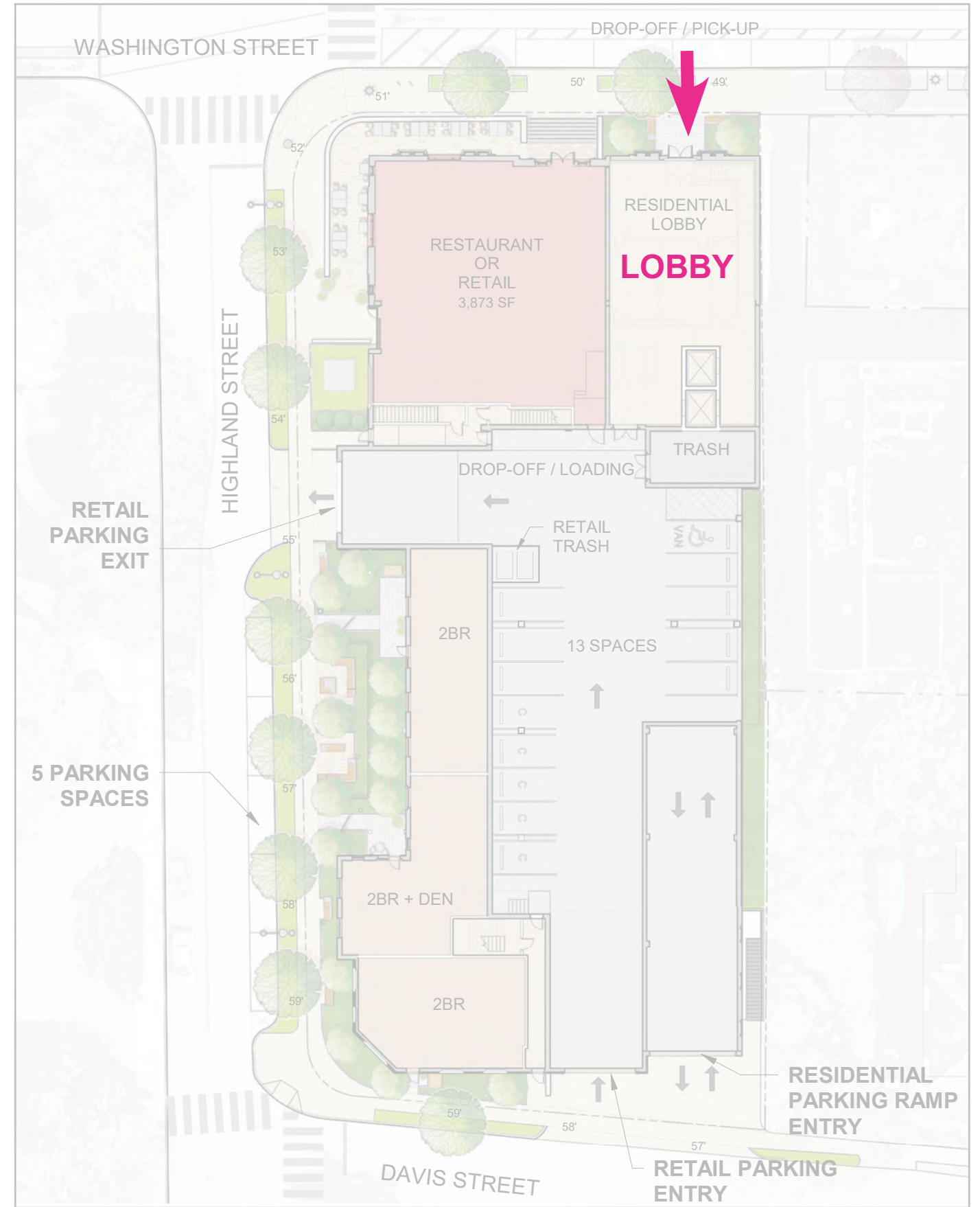


LOBBY LOCATION

ORIGINAL

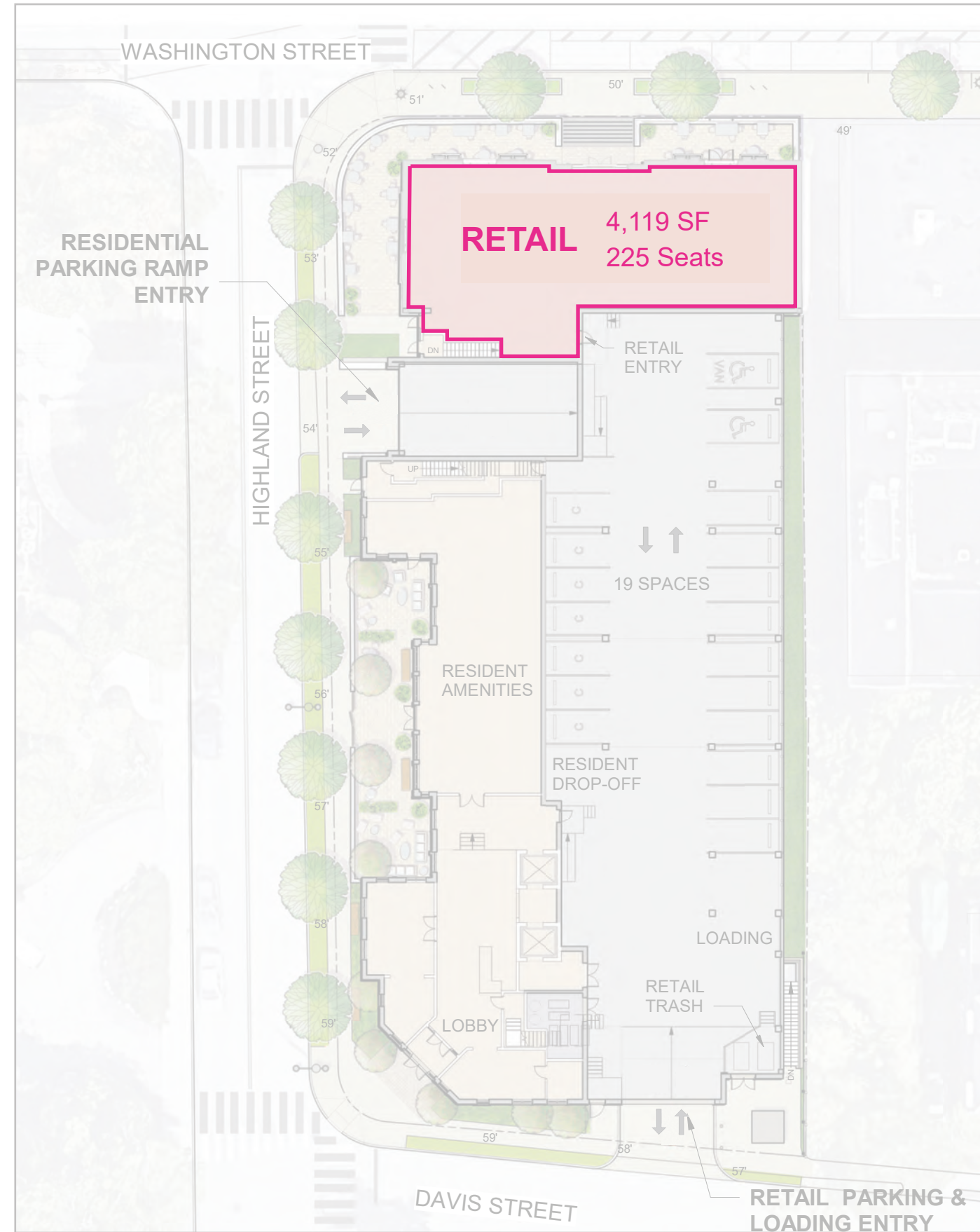


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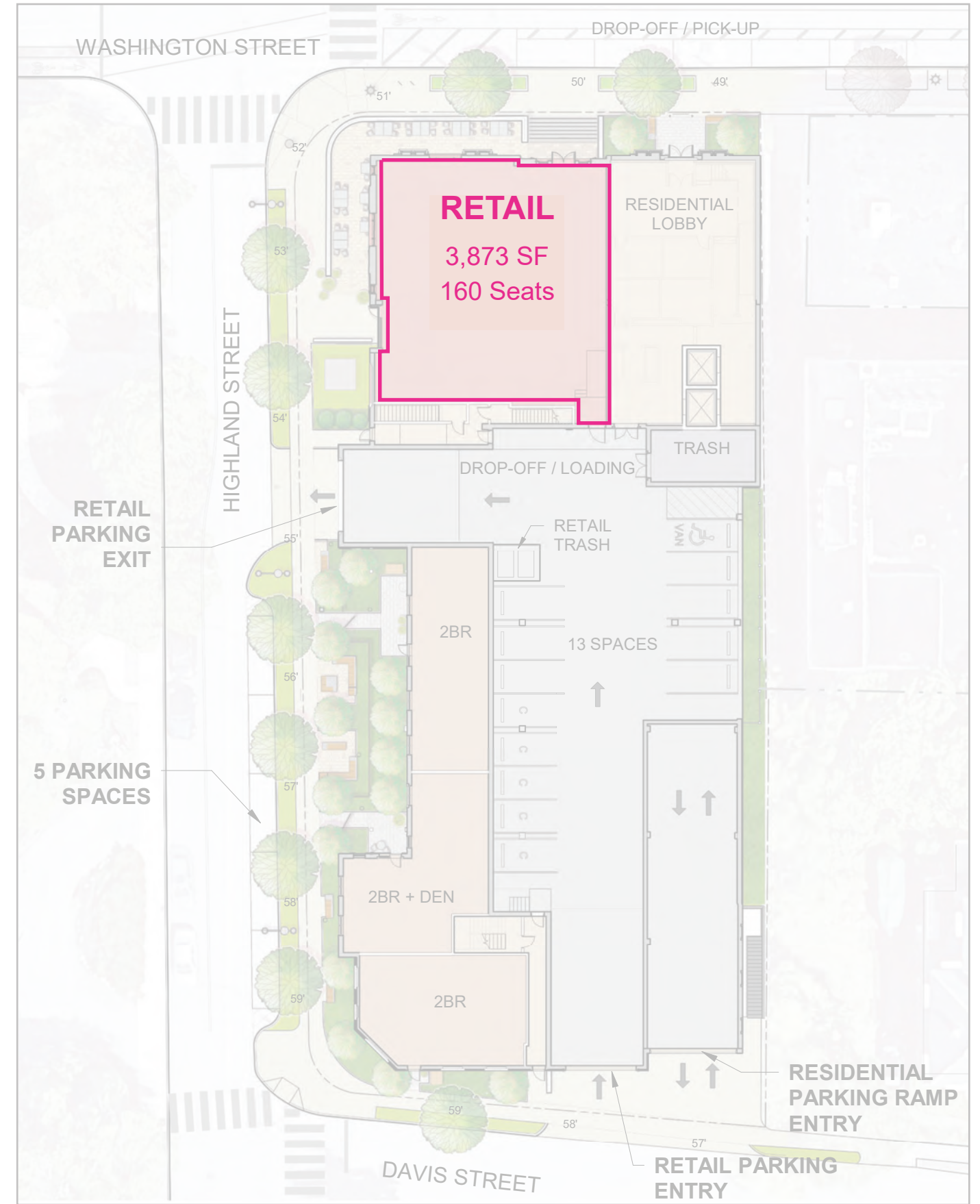


RETAIL REDUCES

ORIGINAL

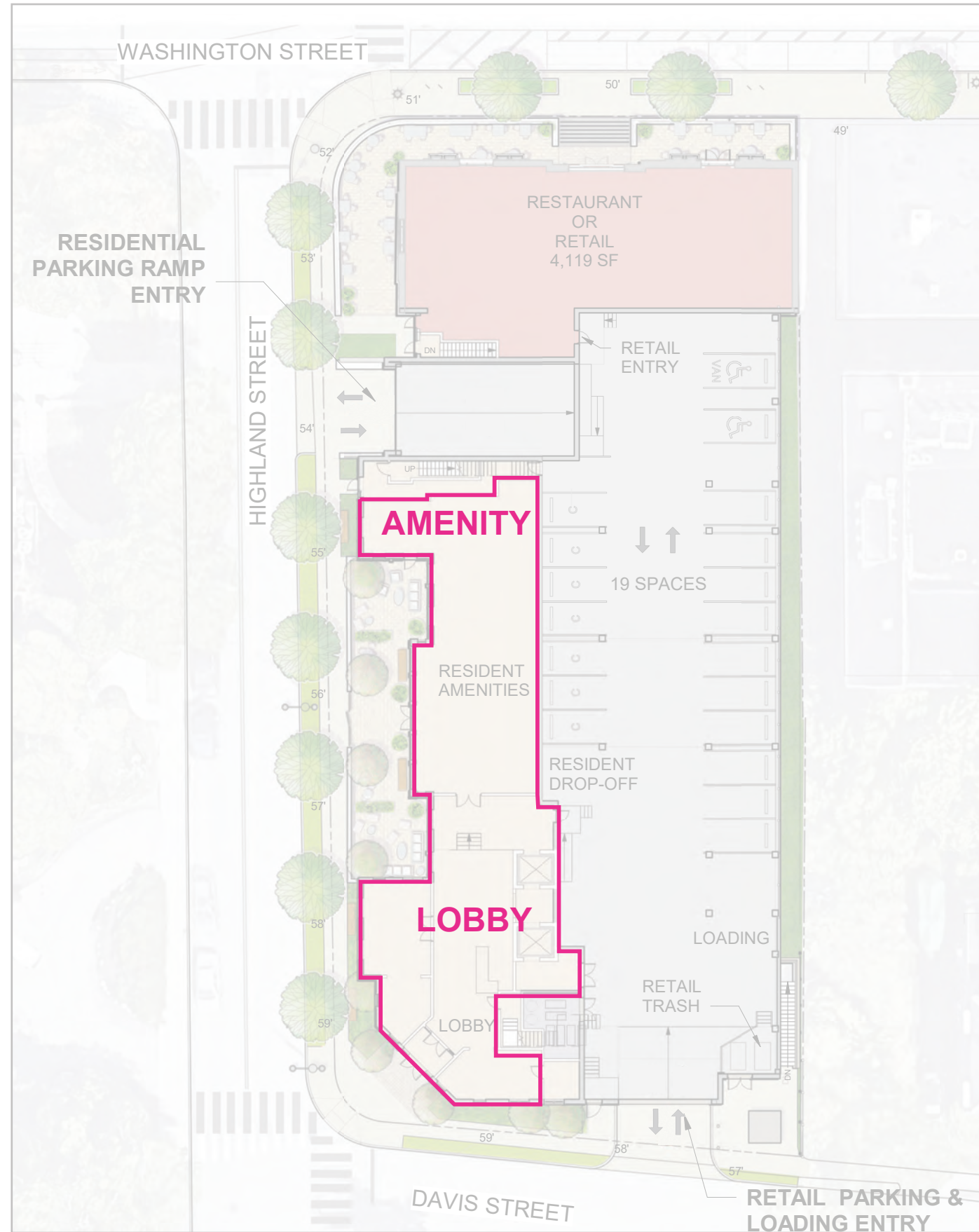


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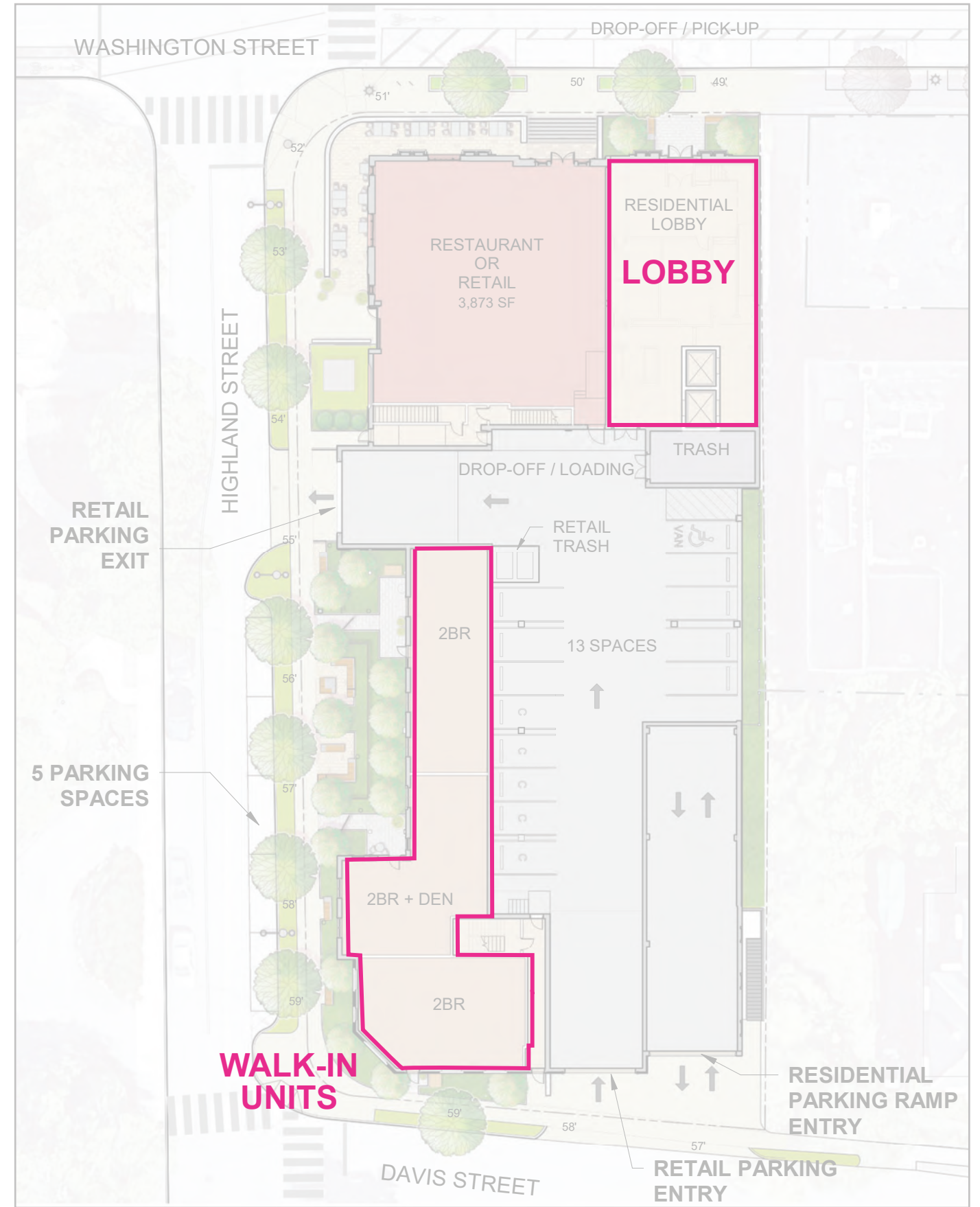
RESIDENTIAL GROUND FLOOR USE

ORIGINAL



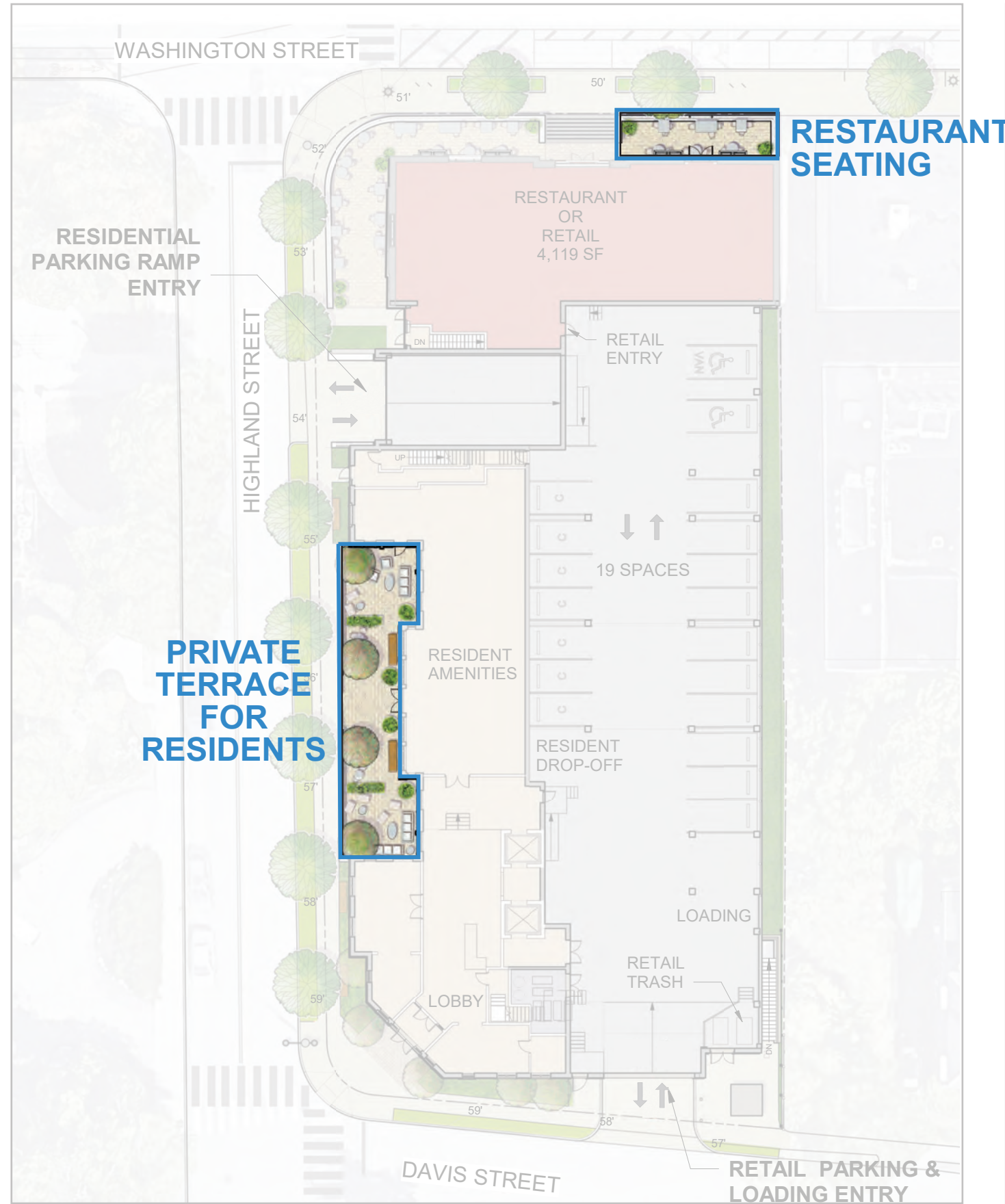
REVISED

Amenity space moves to upper floors.

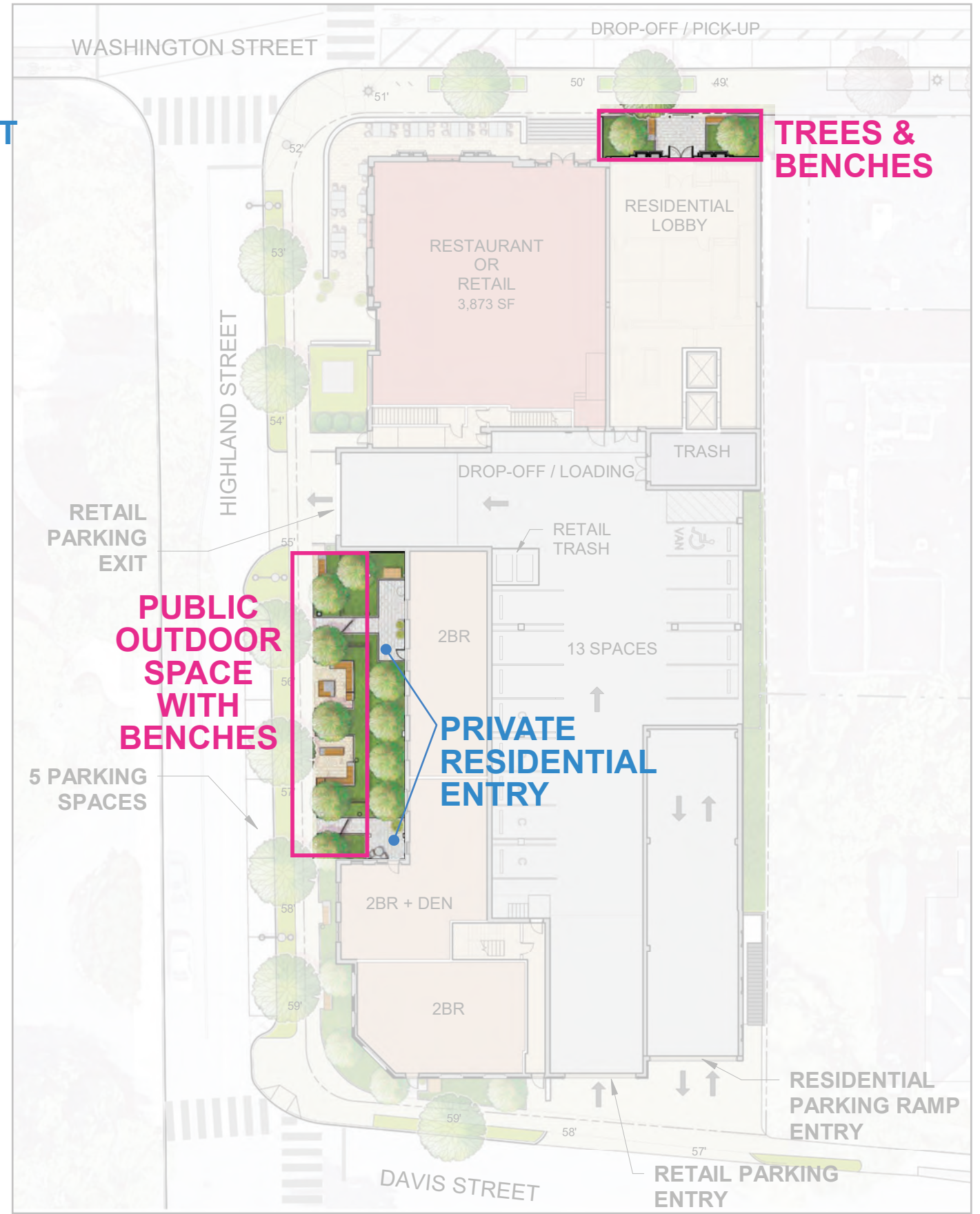


Landscape

ORIGINAL

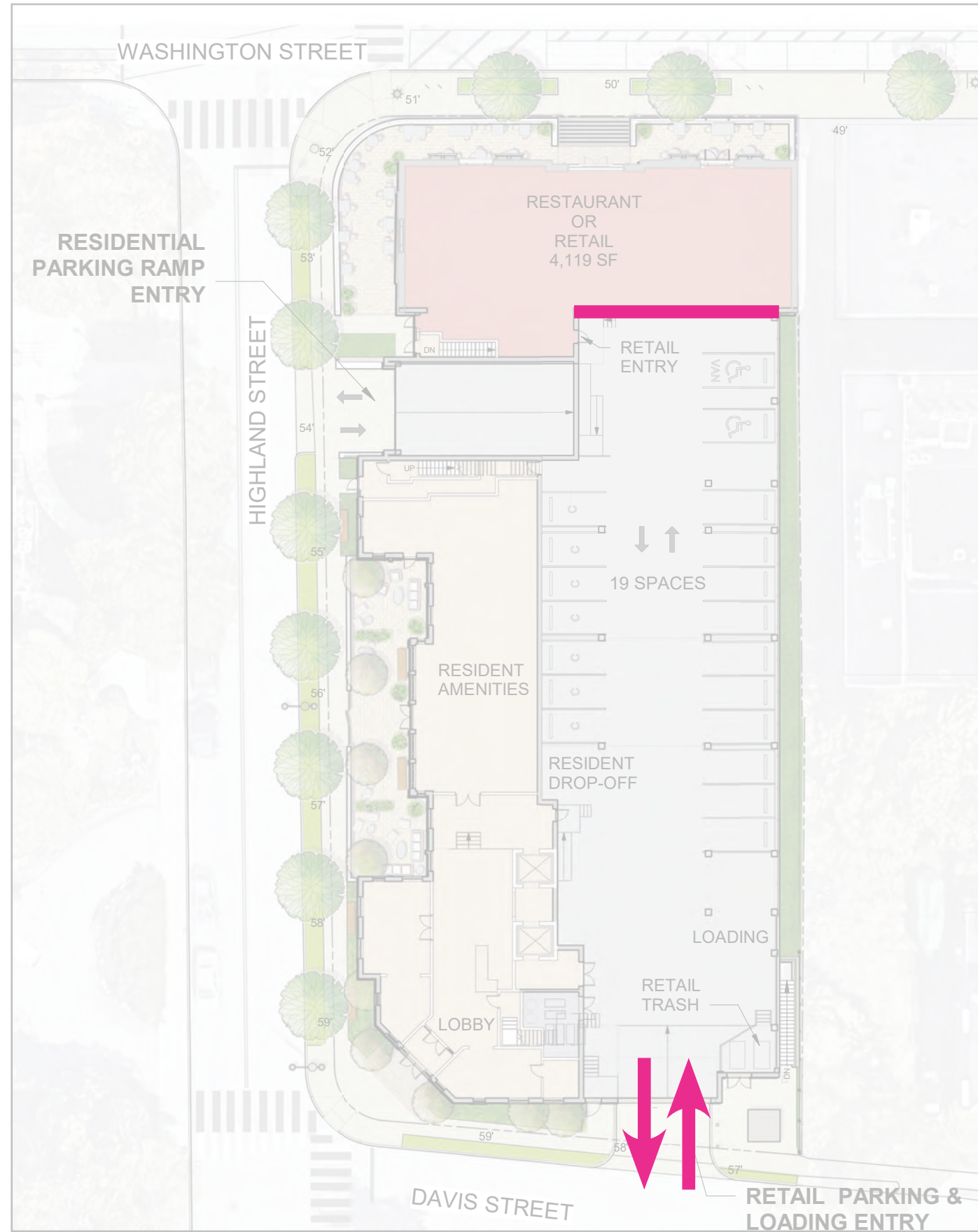


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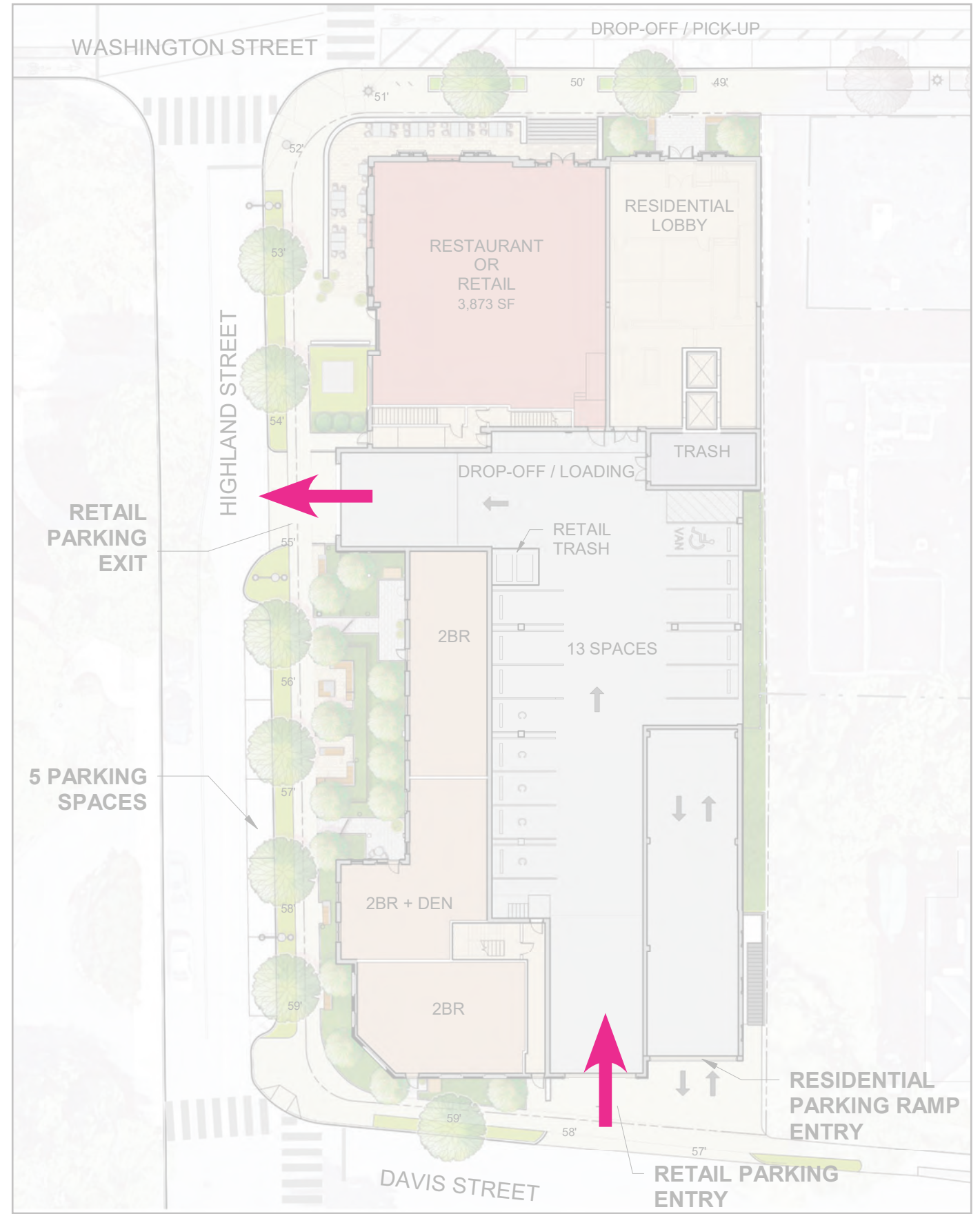


LEVEL 1 PARKING CIRCULATION

ORIGINAL



REVISED

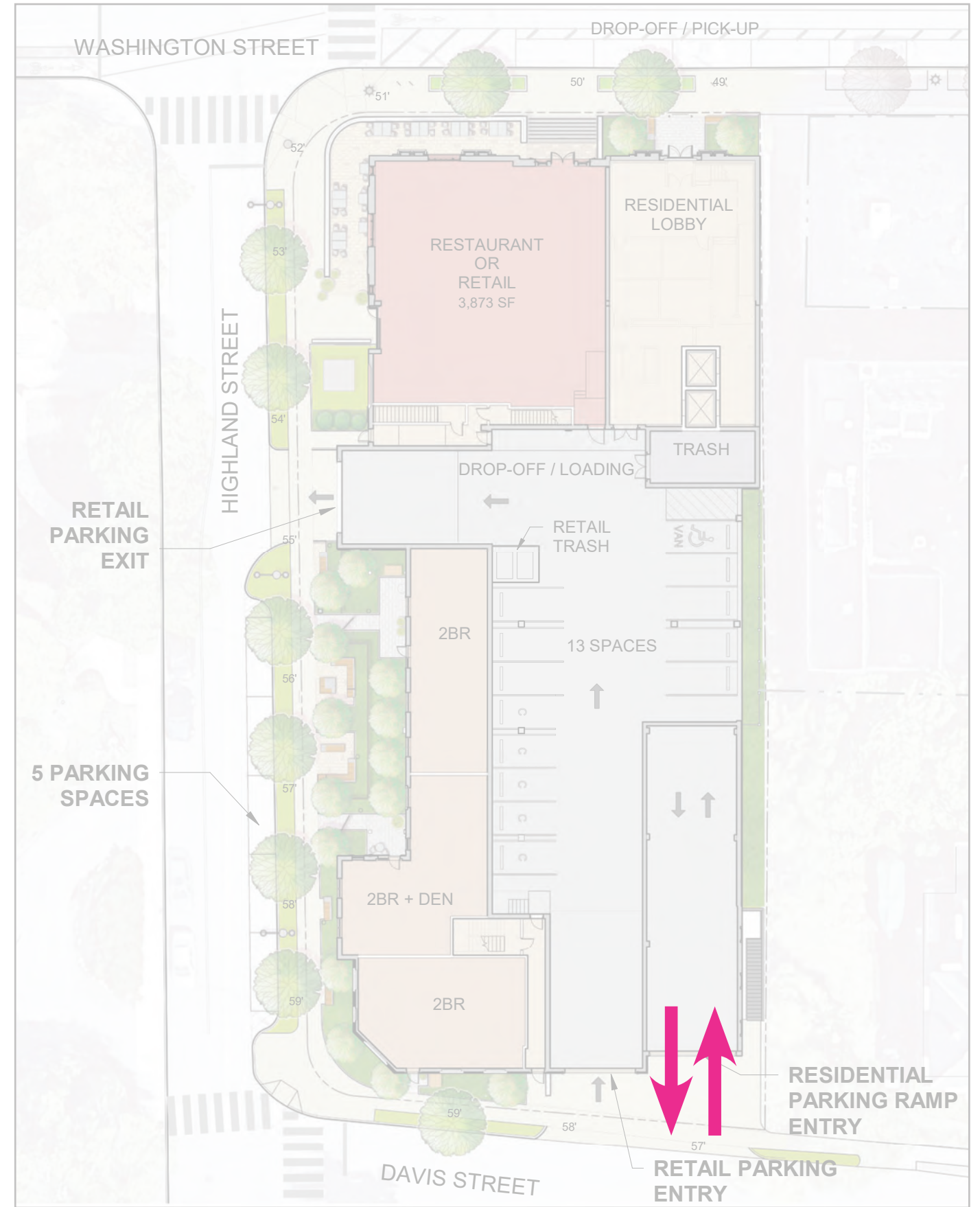


RESIDENT PARKING ENTRANCE

ORIGINAL

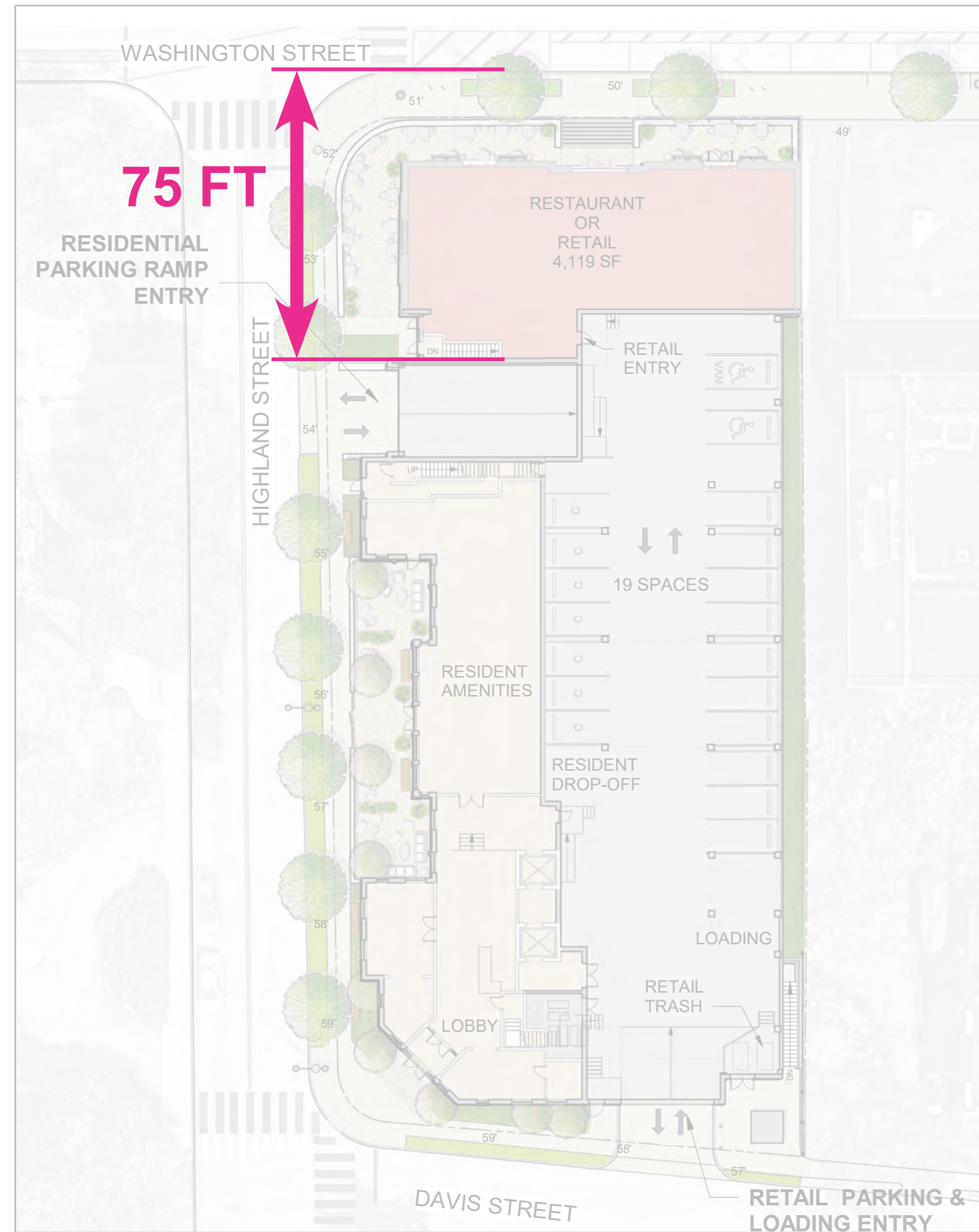


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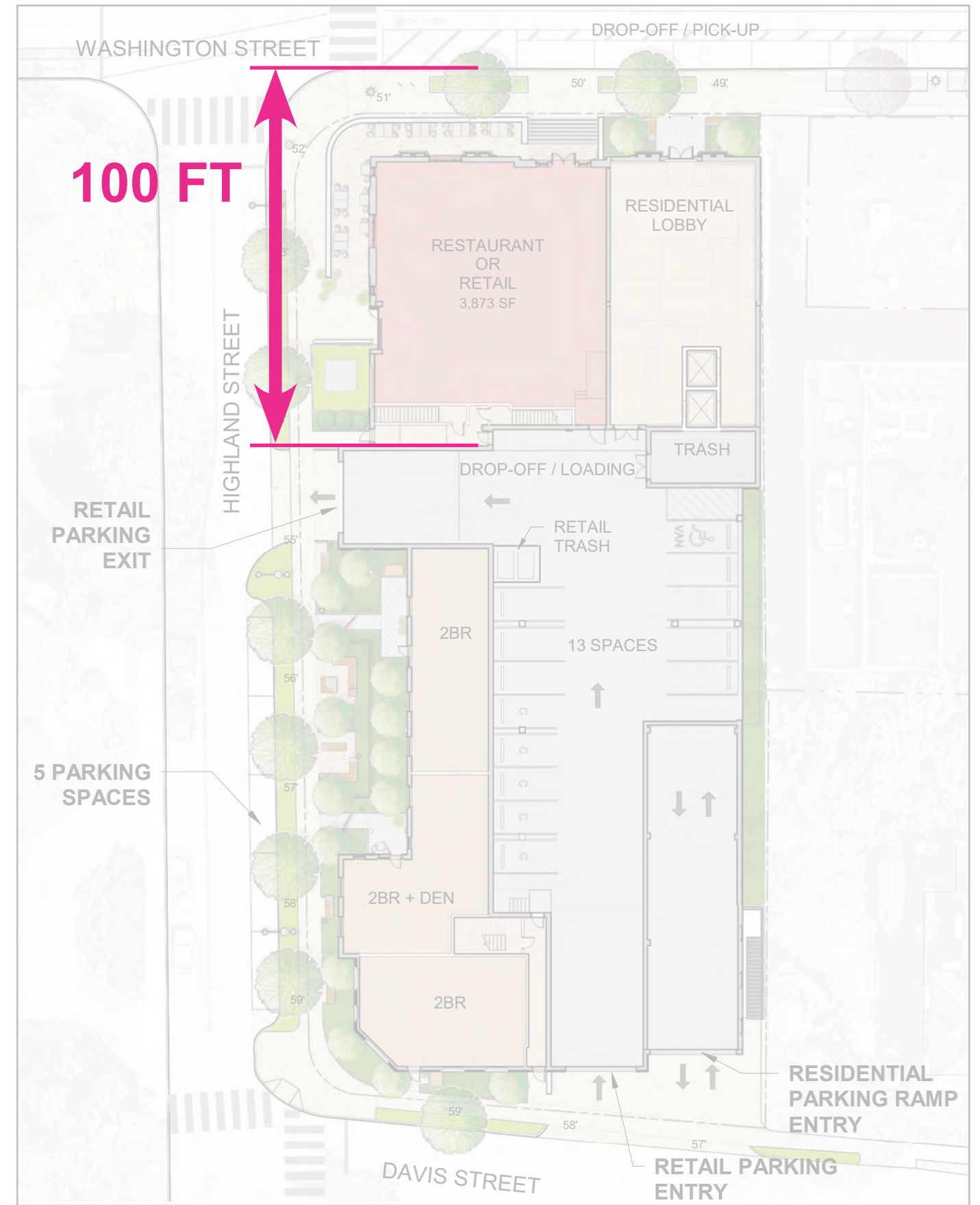


CURB CUT FURTHER FROM INTERSECTION

ORIGINAL

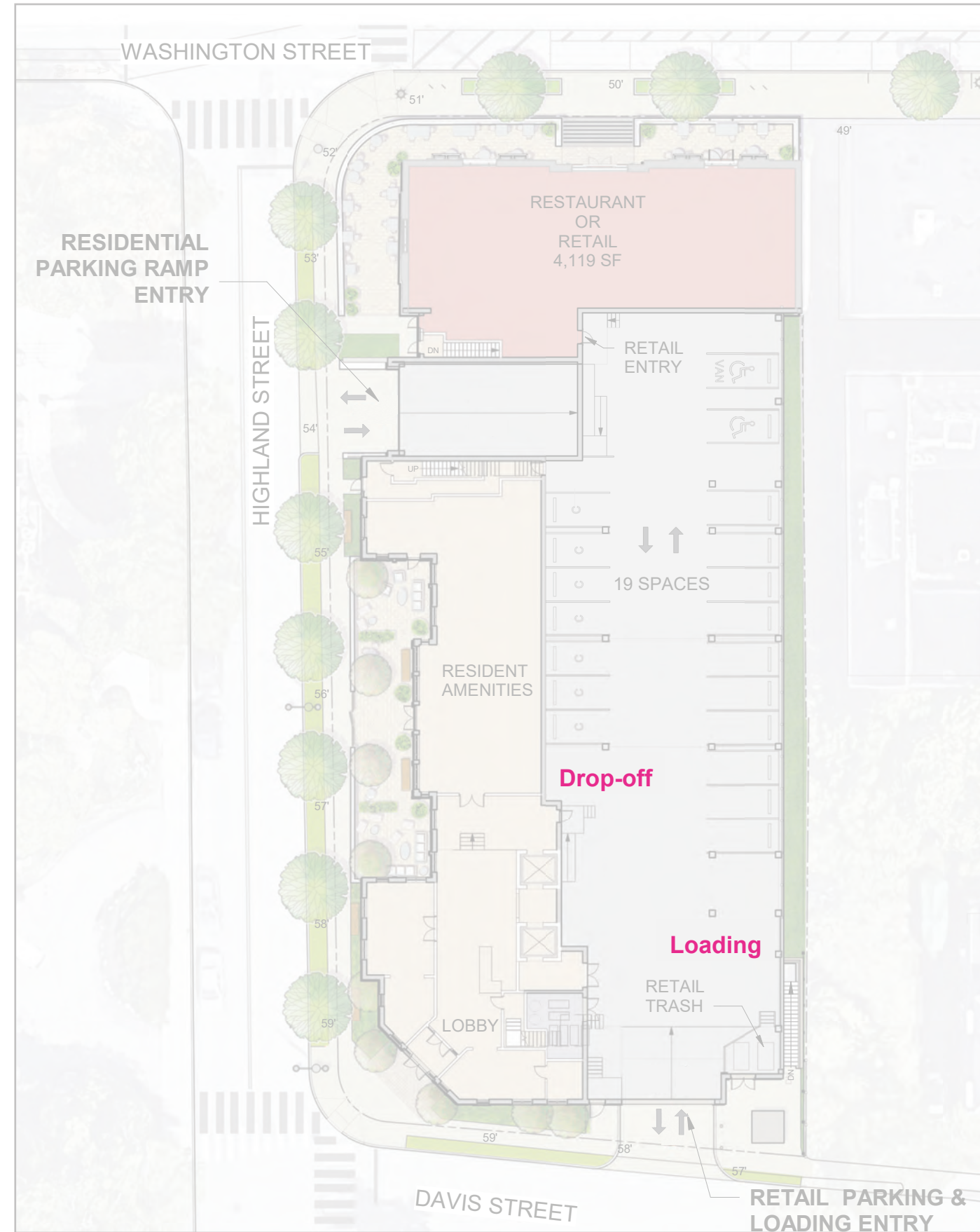


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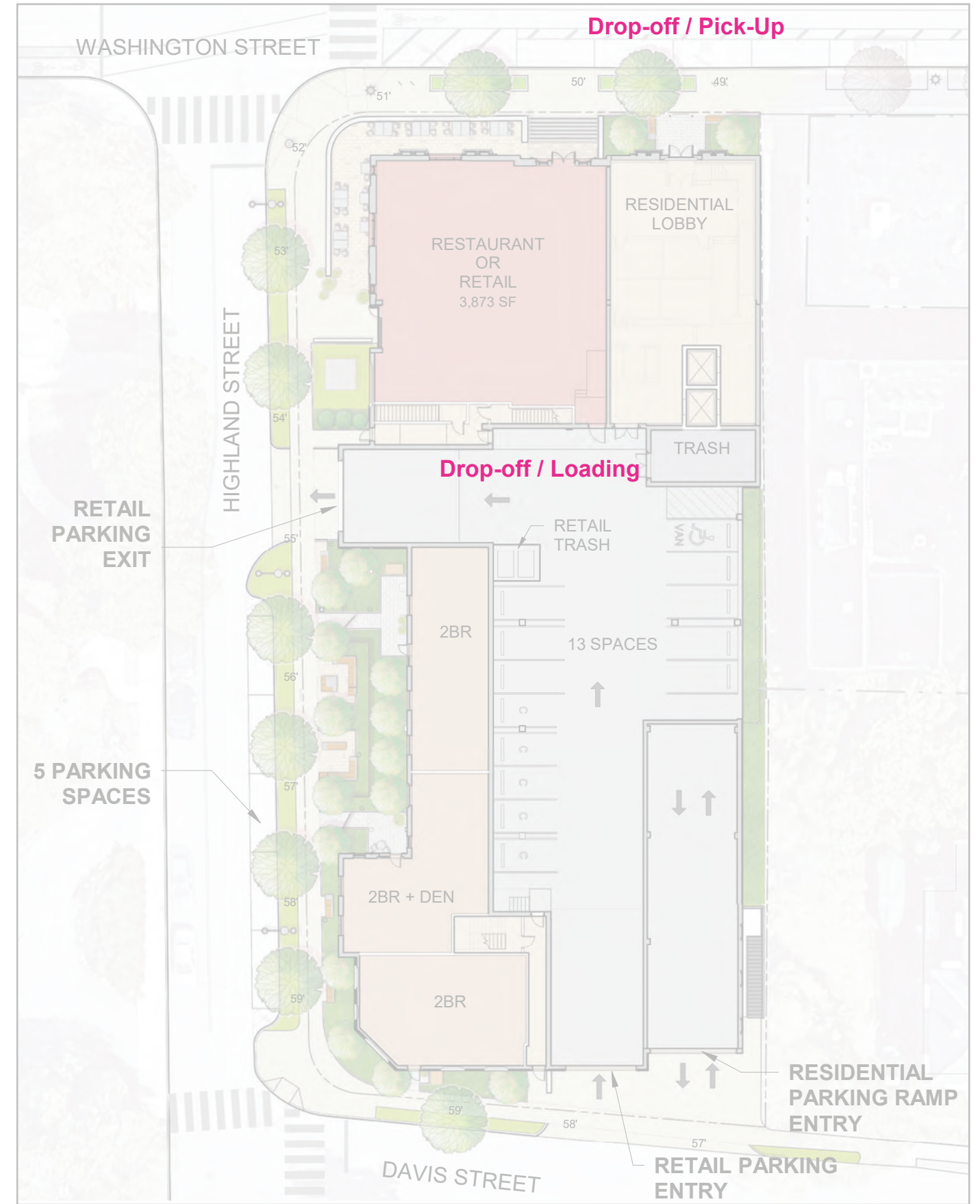


Drop-Off & Pick-Up

ORIGINAL



REVISED





ORIGINAL



REVISED



ORIGINAL



REVISED





Residential Entry View 1

1314 Washington St.



ORIGINAL



REVISED



SUBMITTED



PROPOSED



ORIGINAL



REVISED

Landscape

GROUND FLOOR LEVEL

ORIGINAL

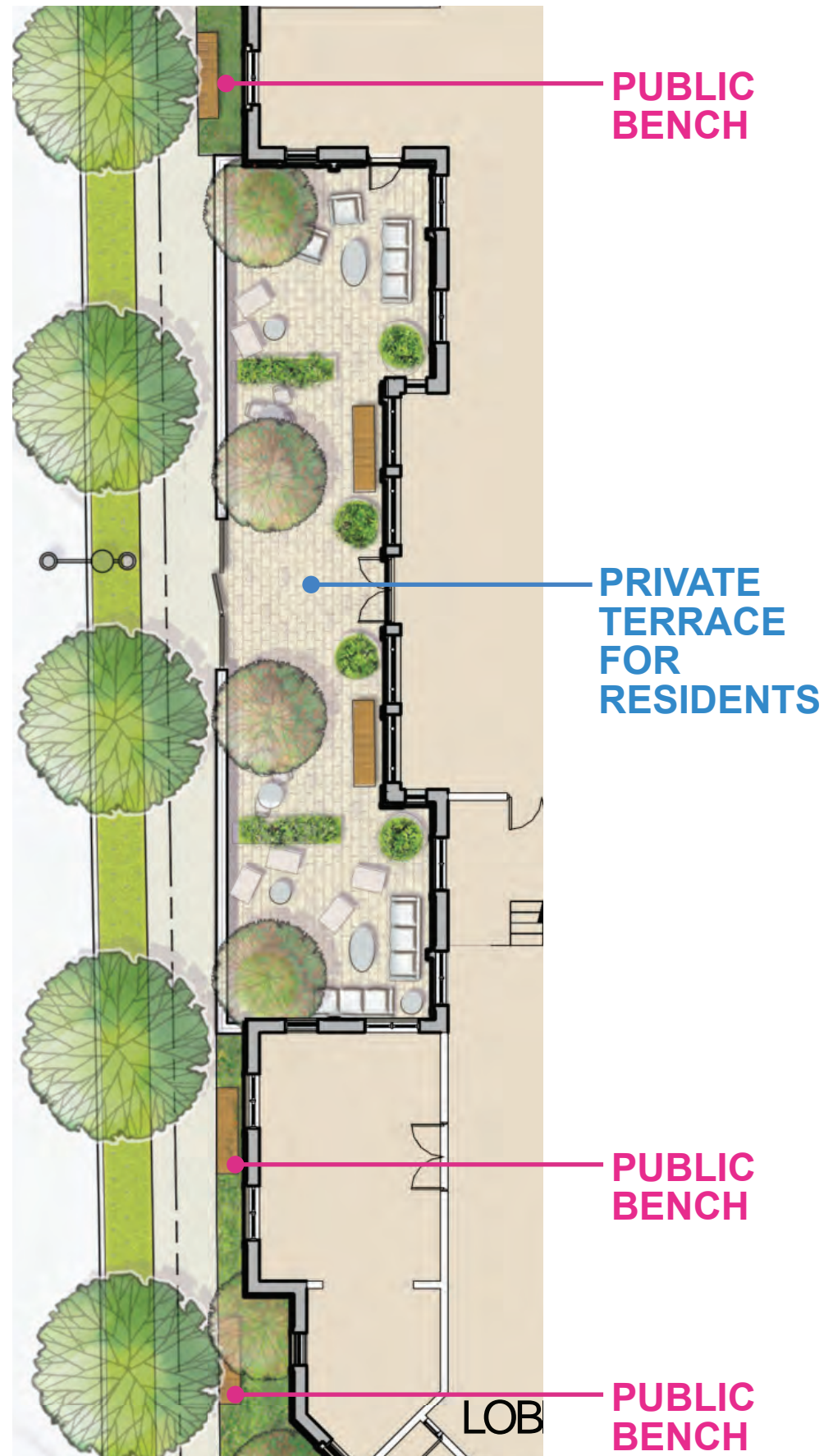


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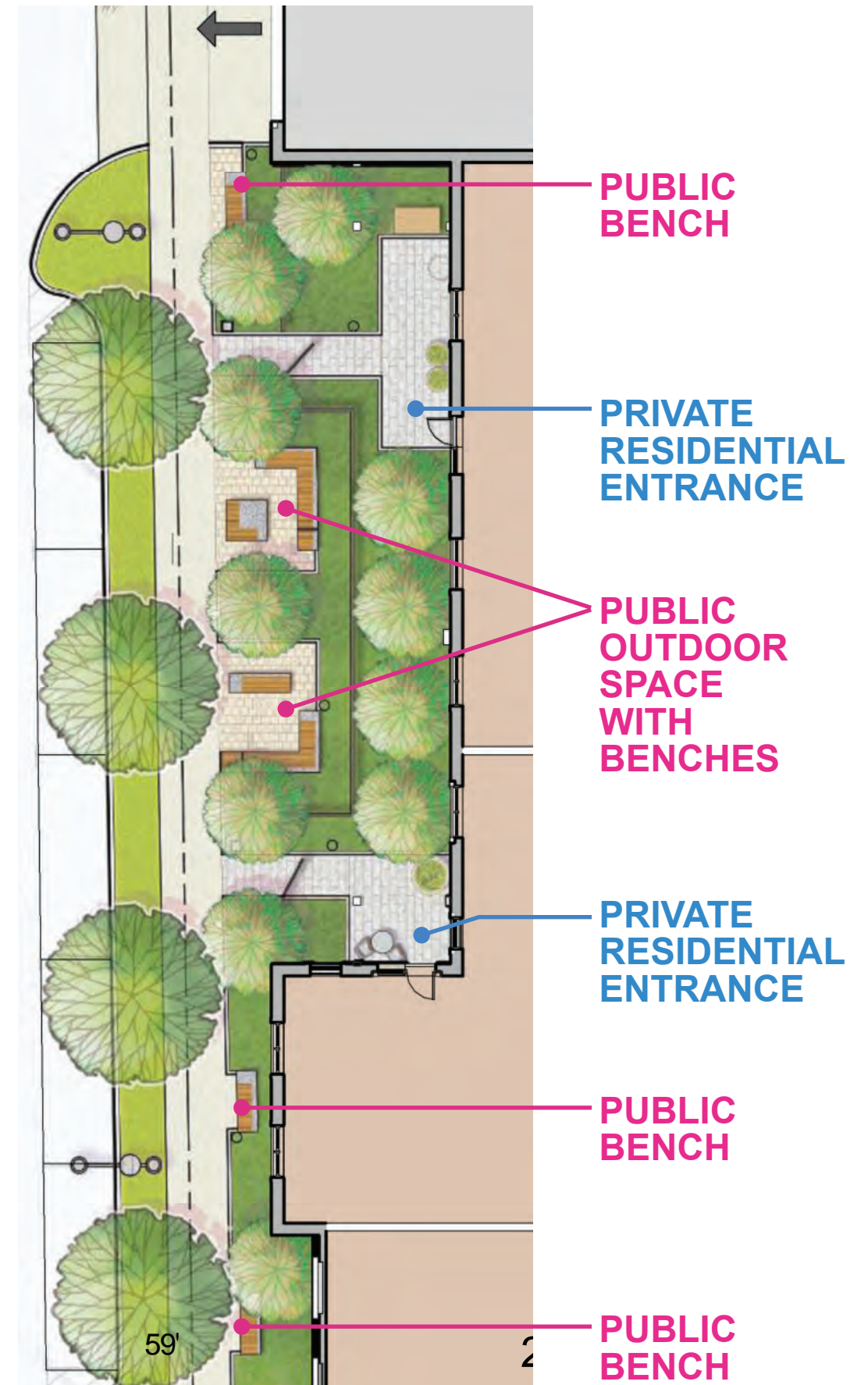


Landscape on Highland Street

ORIGINAL

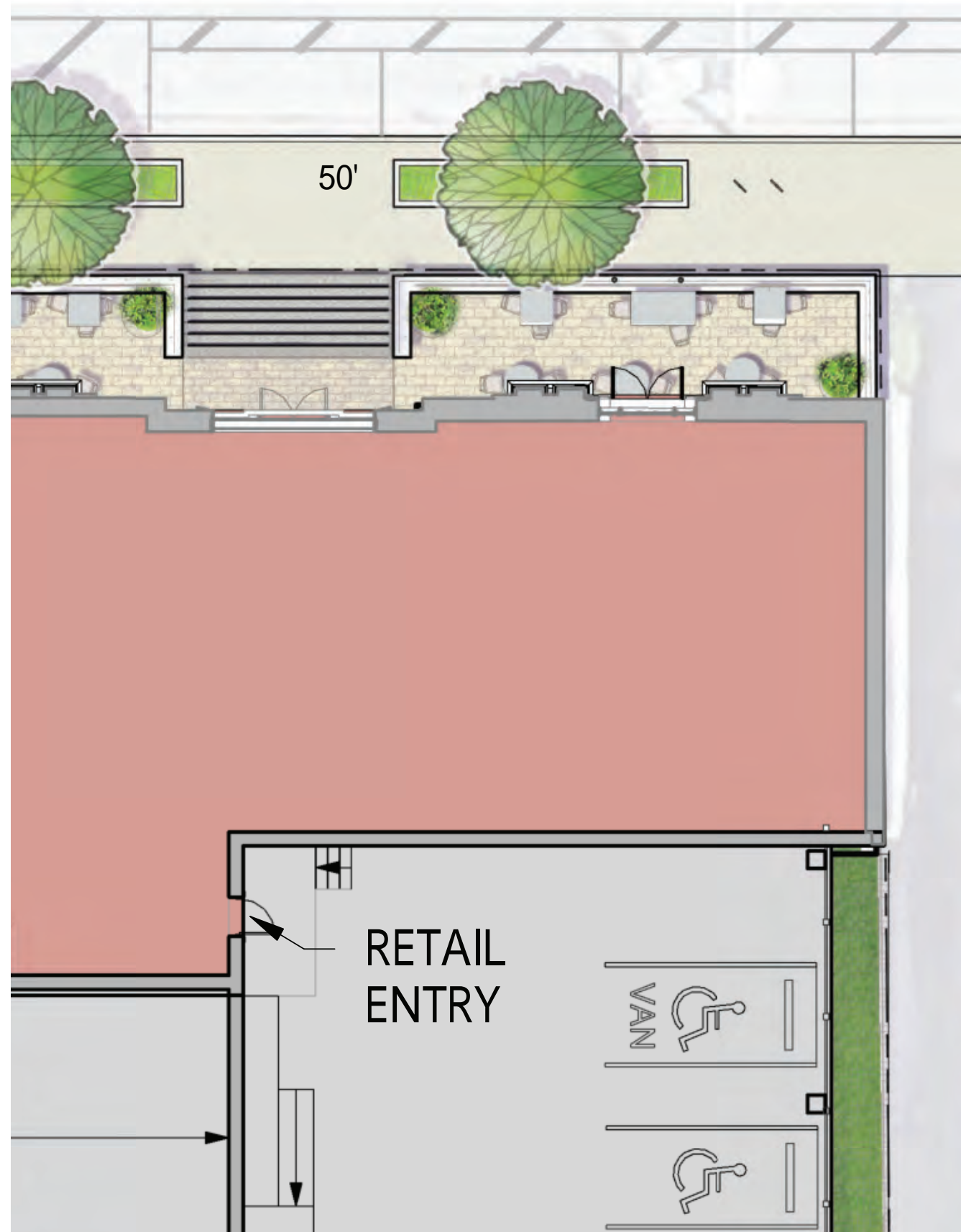


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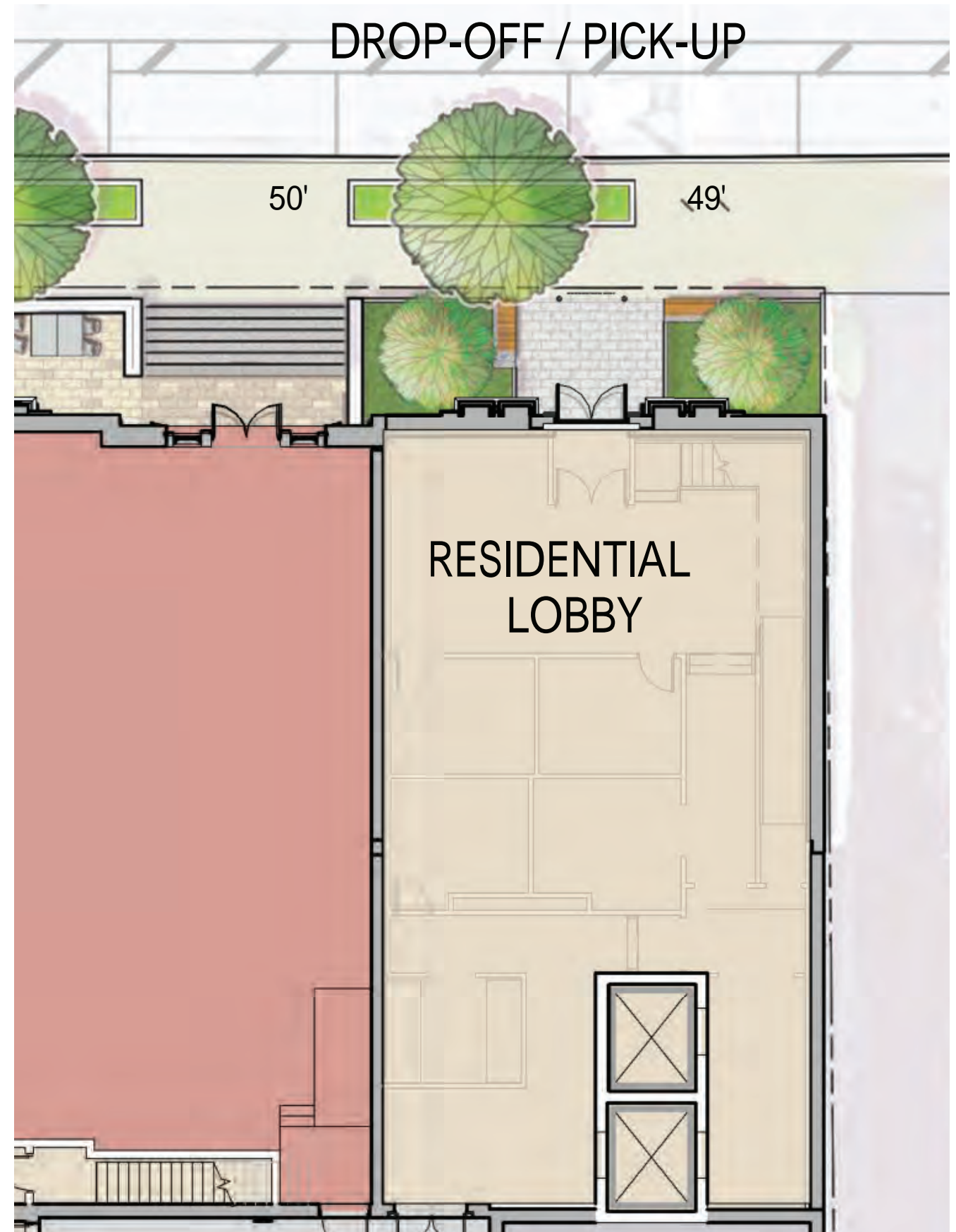


Landscape on Washington Street

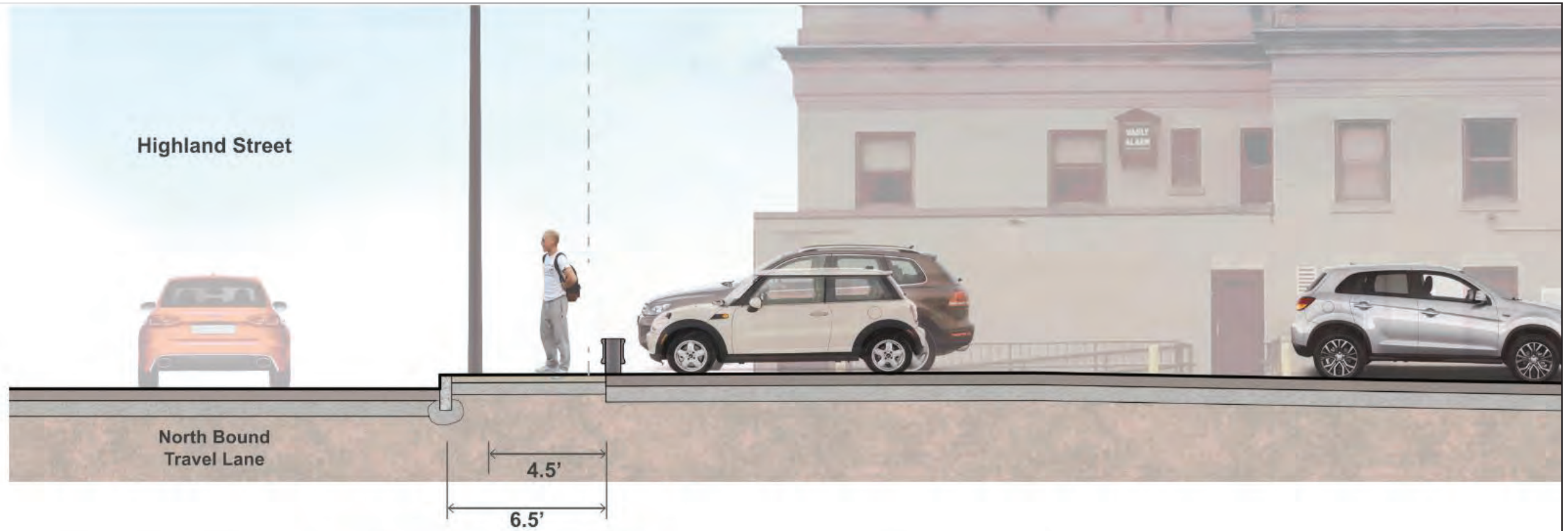
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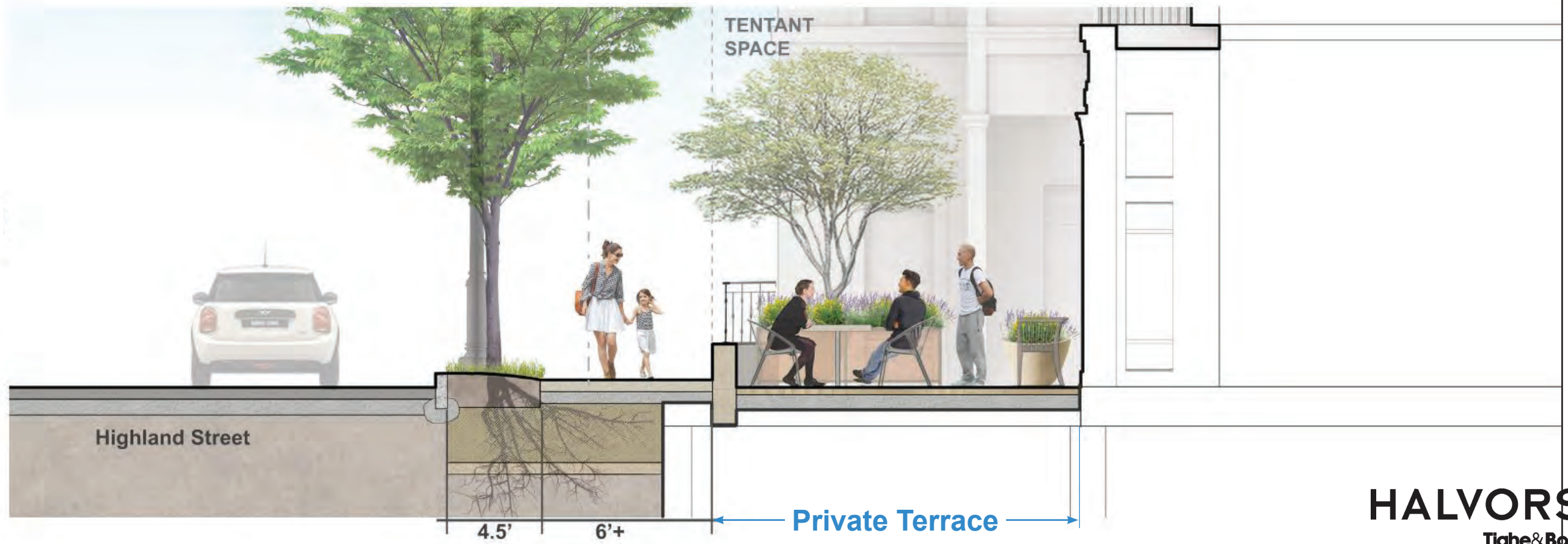
REVISED



Existing



Original



HALVORSON
Tighe&Bond STUDIO

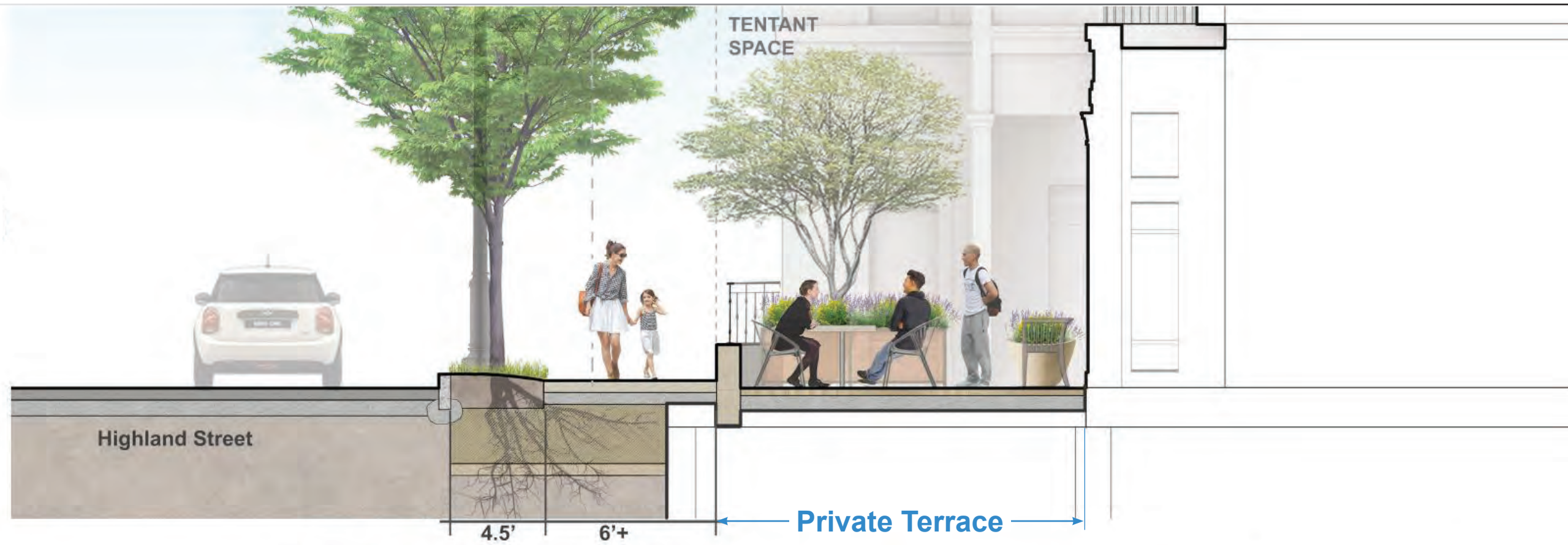


DAVID M. SCHWARZ ARCHITECTS

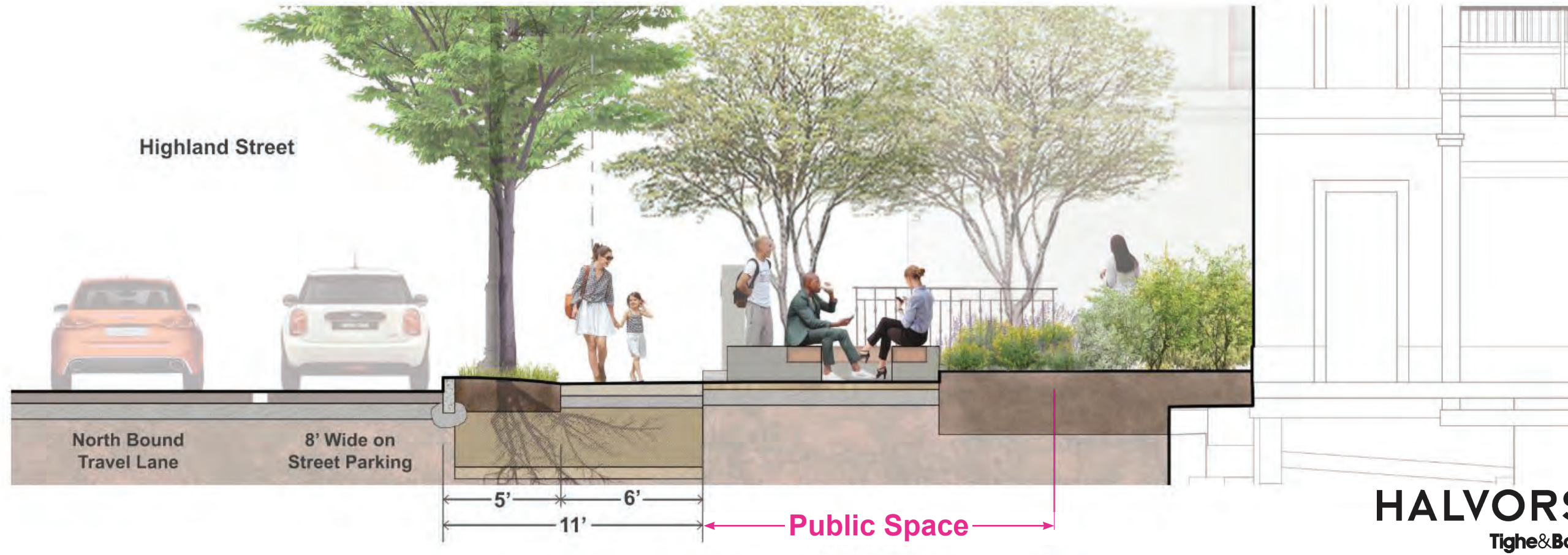
SIDEWALK SECTION

MARK
DEVELOPMENT

Original



Revised



HALVORSON
Tighe&Bond STUDIO

Sustainability

Mark Development is committed to:

- 1. Passive House feasibility studies and energy modeling** to inform design and construction approach.
- 2. LEED BD+C v4 Multifamily Midrise** certifiability at the Gold level.
- 3. Electrification** of the residential portions of the project to reduce fossil fuel dependence.
- 4. Embodied Carbon preservation** by revitalizing a portion of the existing bank building.
- 5. Sustainable Site Design** and ecosystem improvements.
- 6. Electric Vehicle charging stations:** ten on Level P1 and two on Level 1.

Construction Management Plan

Final CMP will include:

- Contact Information
- Hours of Operation (Exterior Construction/Heavy Equipment Operation limited to Weekdays 7AM-5PM and Saturdays 8AM-4PM)
- Schedule
- Site Plan
- Truck Routing
- Site Security and Safety
- Noise/Vibration Monitoring
- Air Quality and Emissions Monitoring
- Rodent Control Plan
- Tree Protection

Transportation

Project Program 1314 Washington Street

Existing:

- 7,887 sf retail

Proposed:

- 50 Apartments
- 3,873 sf of Ground Floor Commercial Space
(Formerly 225 seats; Now revised to 160 seats.)



Project Study Area

\\vhb\gb\proj\Wat-TS\15636.00 WN Santander Residential\Graphics\FIGURES\Aerials.dwg

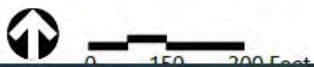
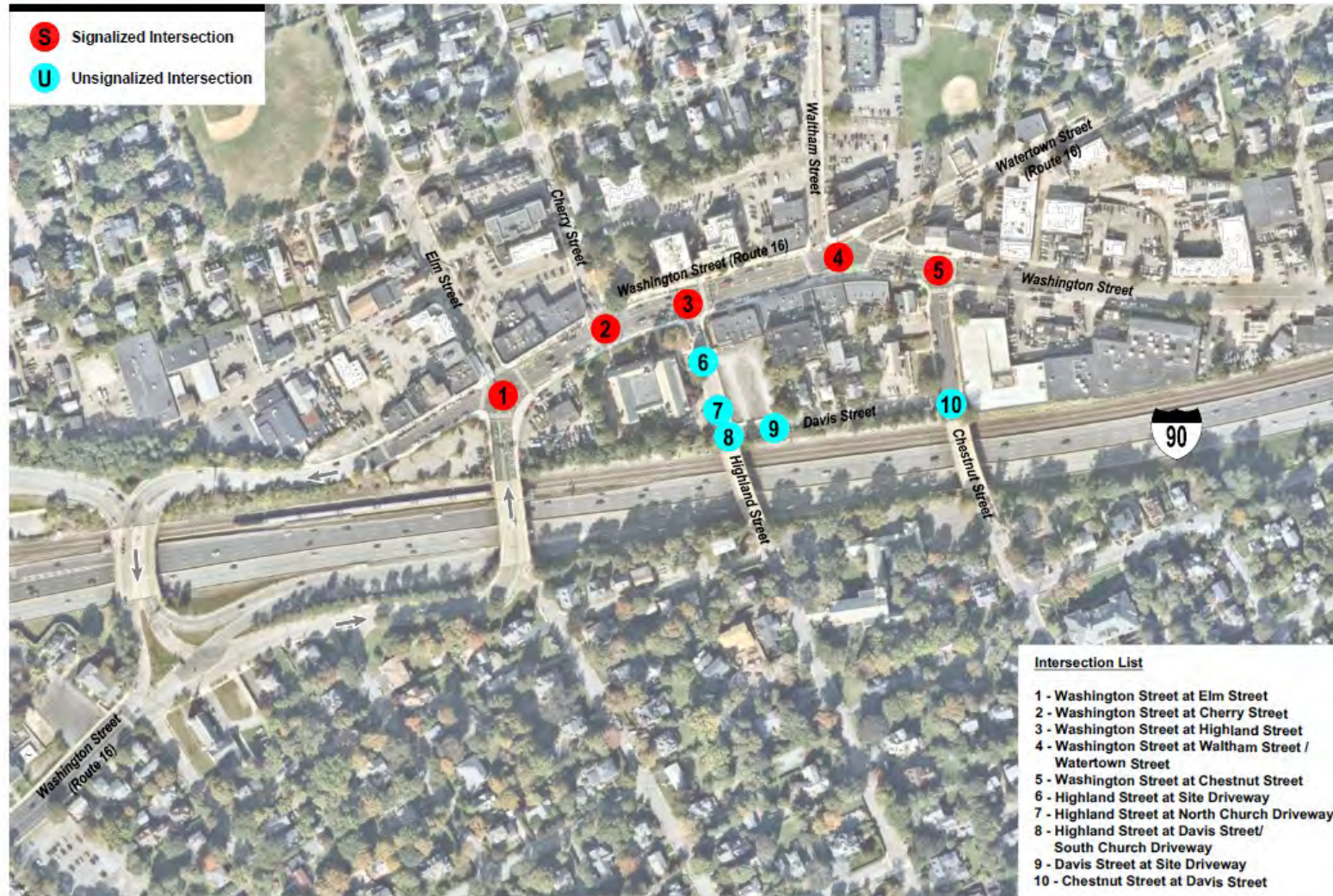


Figure 1
Study Area Intersections



Traffic Generation

Table 7 Project-Generated Vehicle Trips by Use

	Residential ^a	Retail ^b	Total Net New Vehicle Trips ^c	Pass-By ^d	Total New Vehicle Trips w/ Pass-By ^e
Weekday Daily					
Enter	64	69	133	23	156
<u>Exit</u>	<u>67</u>	<u>67</u>	<u>134</u>	<u>23</u>	<u>157</u>
Total	131	136	267	46	313
Weekday Morning					
Enter	2	4	6	1	7
<u>Exit</u>	<u>6</u>	<u>2</u>	<u>8</u>	<u>1</u>	<u>9</u>
Total	8	6	14	2	16
Weekday Evening					
Enter	6	11	17	6	23
<u>Exit</u>	<u>4</u>	<u>9</u>	<u>13</u>	<u>6</u>	<u>19</u>
Total	10	20	30	12	42

a Residential vehicle trips with internal capture and mode share credits applied.

b Retail vehicle trips with internal capture, mode share, and pass-by credits applied.

c Sum of columns a and b.

d Pass-by Credits of 25% and 40% applied to weekday morning and weekday evening peak hour retail trip generation, respectively.

e Sum of columns c and d.

Access and Circulation



Commercial Parking At-Grade (13 Spaces)

Residential Parking Access Underground (50 Spaces and 6 Tandem)



Highland Street On-Street Parking

EXISTING



PROPOSED



On-Street Parking Occupancy Study Area (5 Minute Walkshed)

Figure 1: Parking Occupancy Study Area

West Newton Village | Newton, Massachusetts



Not to Scale

- Study Area Boundary
- Study Area Streets

- 1 – Waltham Street Municipal Parking Lot
- 2 – Cherry Street Municipal Parking Lot
(displayed as eastern area and western area)

Note: Study area includes all on-street parking spaces and municipal parking lots within the study area (does not include privately-owned parking lots).



On-Street Parking Occupancy Study Area

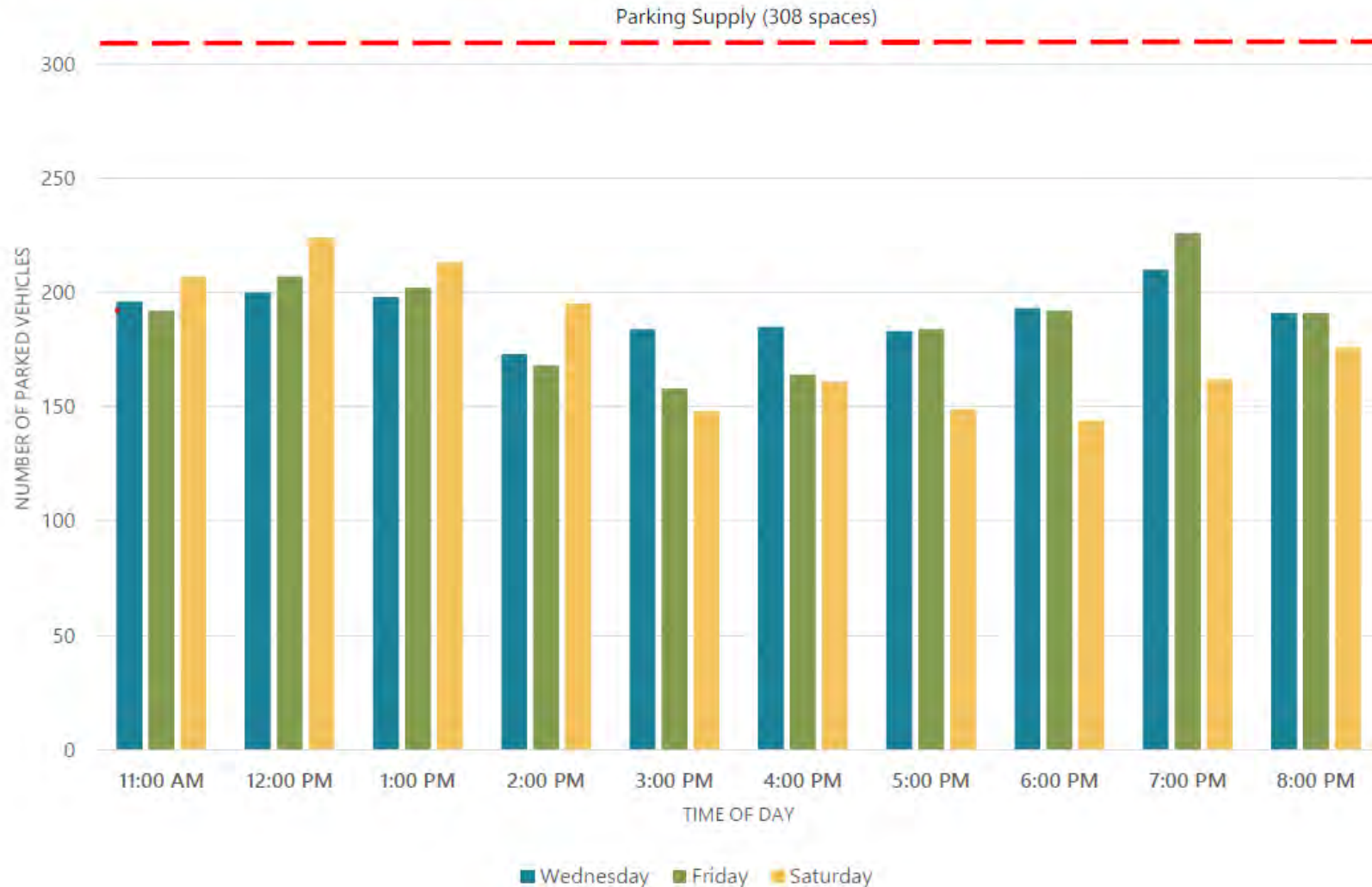
Observations Were Conducted During The Following Four Time Periods:

- **Typical Weekday; Wednesday September 21, 2022, between 11:00 AM and 9:00 PM**
- **Typical Friday: September 16, 2022, between 11:00 AM and 9:00 PM**
- **Typical Saturday: September 24, 2022, between 11:00 PM and 9:00 AM**



Parking Occupancy (5-minute walkshed)

Exhibit 1 West Newton Village Public Parking Occupancy (September 2022)



- **Peak Wednesday**
210 parked at 7PM
(98 avail)
- **Peak Friday** 226
parked at 7pm (82
avail)
- **Peak Saturday** 224
parked 12P M (84
avail)



Councilor Questions:

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- “Why weekend nights in August were chosen for your parking analysis when I think it is common knowledge that this is a relatively quiet time in the city. Can another analysis be done in October”.

Plan was always to get both summer and fall counts update was conducted in Sept as noted

- “Can you provide us with a map of the area that was considered when determining available parking”.

Map was presented in previous slide

- “Were any counts taken of the number of cars in the Santander parking lot at the time of the analysis? If so, how many cars were parked in that lot.. if you do another analysis, I would ask that the number of cars parked in that lot be counted as well”.

Santander Bank Driveways were chained off during parking counts



Transportation Demand Management (TDM)

- **\$10,000 contribution to Bike Share**
- **\$35,000 contribution to Bus Shelter**
- **\$15,000 contribution to Transit Subsidy (for employees and residents)**

- **Parking (Market rate, unbundled)**
- **Bike Parking (1 bike space per unit)**
- **TDM Coordinator & TMA Membership**

